



COMPREHENSIVE PARKING PLAN TOWN OF SURFSIDE BEACH

JANUARY, 2010

DDC PN 09.09306L



TABLE OF CONTENTS

1.	EXECUTIVE SUMMARY	Page 1-5
	1.1 Introduction	
	1.2 Previous Study	
	1.3 Key Findings	
	1.4 Recommended Course of Action	
2.	EXISTING CONDITIONS	Page 1-14
	2.1 Introduction	
	2.2 Location and History	
	2.3 Study Area	
	2.4 Analysis	
	2.5 Section Summary	
3.	EXISTING DATA	Page 1-3
	3.1 Introduction	
	3.2 Supply and Demand	
	3.3 Parking Violations	
	3.4 Parking Revenue	
	3.5 Section Summary	
4.	STREETScape AND SIGNAGE	Page 1-8
	4.1 Introduction	
	4.2 Road Characteristics	
	4.3 Ocean Boulevard	
	4.4 Downtown Area	
	4.5 Signage	
	4.6 Section Summary	
5.	CONCLUSIONS AND RECOMMENDATIONS	Page 1-25
	5.1 Introduction	
	5.2 Existing Measures	
	5.3 Future Implementation	
6.	RECOMMENDED COURSE OF ACTION.....	Page 1-5
	6.1 Introduction	
	6.2 Phase One	
	6.3 Phase Two	
	6.4 Phase Three	
	6.5 Phase Four	
	6.6 Phase Five	
	6.7 Phase Six	

GRAPHICS

IMAGES

Image 2.2-1 Existing Surfside Pier
Image 2.3-1 Existing Surfside Pier
Image 2.4.1-1 Downtown Surfside Drive
Image 2.4.1-2 16th Avenue North
Image 2.4.1-3 5th Avenue North
Image 2.4.1-4 Surfside Drive
Image 2.4.1-5 10th Avenue South
Image 2.4.1-6 Melody Lane
Image 2.4.2-1 Golf Cart User
Image 2.4.2-2 Golf Cart Parking
Image 2.4.3-1 Bicycle User
Image 2.4.4-1 Sidewalk User
Image 2.4.4-2 Ocean Boulevard Pedestrian Crosswalk
Image 2.4.4-3 Existing Drainage Swale

Image 3.2-1 Surfside Drive Parking Lot
Image 3.2-2 Yaupon Drive Parking Lot
Image 3.2-3 16th-17th Avenues North Parking Lot
Image 3.2-4 Street End Parking Lot
Image 3.2-5 13th Avenue South Parking Lot

Image 4.2-1 Intersection of Surfside Drive and US Highway 17 Business
Image 4.2-2 Hardscape Feature
Image 4.3-1 North Ocean Boulevard 40' R.O.W.
Image 4.3-2 South Ocean Boulevard 40' R.O.W.
Image 4.3-3 North Ocean Boulevard 75' R.O.W.
Image 4.3-4 South Ocean Boulevard 75' R.O.W.
Image 4.4-1 Existing Downtown Surfside Drive
Image 4.4-2 Existing Downtown Surfside Drive
Image 4.4-3 Existing Downtown Surfside Drive
Image 4.5-1 Existing Signage

Image 5.2.1-1 Downtown Façade "Before"
Image 5.2.1-2 Downtown Façade "After"
Image 5.2.2-1 Metered Parking Lot
Image 5.2.3-1 Vehicles Parked in Prohibited Area
Image 5.2.4-1 Sand Street End
Image 5.2.4-2 Paver Street End
Image 5.2.5-1 Corner of Surfside Drive and Poplar Drive
Image 5.2.6-1 Amenity Area
Image 5.3.1-1 Existing Sidewalk
Image 5.3.1-2 Existing Sidewalk
Image 5.3.2-1 Bollards
Image 5.3.2-2 Bollards
Image 5.3.2-3 Curb and Gutter
Image 5.3.2-4 Street Trees
Image 5.3.3-1 Legends in Concert Parking Lot
Image 5.3.3-2 Legends in Concert Parking Lot
Image 5.3.4-1 Dogwood Drive Ball Field
Image 5.3.4-2 Surfside Drive Amenity Area

GRAPHICS

Image 5.3.4-3 River City Cafe' Parking Lot
Image 5.3.4-4 3rd Avenue South
Image 5.3.5-1 Existing Bicycle Rack
Image 5.3.5-2 East Coast Greenway Route
Image 5.3.6-1 Existing Signage
Image 5.3.6-2 Current Town Project
Image 5.3.7-1 Parking Garage with Retail Façade
Image 5.3.7-2 Parking Garage with Retail Façade

CHARTS

Chart 2.4.1-1 Existing Road and Metered Parking
Chart 2.4.2-1 Existing Golf Cart Parking

Chart 3.3-1 Parking Violations
Chart 3.4-1 Parking Revenue

Chart 5.3.2-1 Parking Control Measures

ELEVATIONS

Elevation 4.3-1 Ocean Boulevard 40' R.O.W.
Elevation 4.3-2 Ocean Boulevard 40' R.O.W.
Elevation 4.3-3 Ocean Boulevard 40' R.O.W.
Elevation 4.3-4 Ocean Boulevard 75' R.O.W.
Elevation 4.3-5 Ocean Boulevard 75' R.O.W.
Elevation 4.3-6 Ocean Boulevard 75' R.O.W.

EXHIBITS

Exhibit 1.1-1 Zoning Map
Exhibit 1.1-2 Aerial Map

Exhibit 2.2-1 Location Map
Exhibit 2.3-1 Study Area
Exhibit 2.4.1-1 Existing Thru Roads
Exhibit 2.4.1-2 Existing Metered Parking
Exhibit 2.4.4-1 Existing Sidewalks

Exhibit 4.4-1 Conceptual Streetscape Plan

Exhibit 5.2.5-1 Surfside Drive and Poplar Drive Parking and Park Plan
Exhibit 5.2.6-1 Library Expansion and Amenity Area Plan
Exhibit 5.3.1-1 Proposed Sidewalks
Exhibit 5.3.2-1 Parking Control Plan
Exhibit 5.3.3-1 Proposed Shuttle Route
Exhibit 5.3.4-1 Dogwood Drive Ball Field Parking Plan
Exhibit 5.3.4-2 Surfside Drive Amenity Area Parking Plan
Exhibit 5.3.4-3 River City Cafe' Shared Parking Plan

GRAPHICS

Exhibit 5.3.4-4 3rd Avenue South Parking Plan

Exhibit 5.3.5-1 Proposed Bicycle Route

Exhibit 5.3.7-1 Retail Space and Parking Garage Plan

Exhibit 6.1-1 Course of Action - Phasing Plan

1. EXECUTIVE SUMMARY

1.1 Introduction

DDC Engineers, Inc. and The LandArt Company, LLC were commissioned by the Town of Surfside Beach, South Carolina to complete a comprehensive parking plan and publish a report for the downtown and beach-side areas of the Town. The Town Council, Staff, local business owners, residents and visitors have all identified parking as one of the top issues currently adversely affecting the Town. The intent of this plan is to identify existing parking and transportation infrastructure, including the obvious opportunities and constraints for efficient use of vehicles, golf carts, bicycles and pedestrians, and to suggest improvements that will help the Town of Surfside Beach better identify, manage, maintain and use the assets of the Town in the future.

Information for this plan was acquired from many sources including the Comprehensive Parking Plan, previously prepared by Waccamaw Regional Planning Council of Governments (COG) dated June 2003, digital video and photography taken from the air as well as on the ground during the month of July 2009, GIS data provided by Horry County and DDC Engineers, Inc., parking data provided by the Town of Surfside Beach, a zoning map and CAD information provided by the Earthworks Group, Inc. dated April 2008 (*Exhibit 1.1-1 Zoning Map*) and through the use of overall aerial information (*Exhibit 1.1-2 Aerial Map*).

1.2 Previous Report

A Comprehensive Parking Plan was previously completed for and provided to the Town of Surfside Beach in June of 2003. The report included analysis of existing and potential parking areas, golf cart, bicycle and pedestrian routes, current and future parking demands and parking revenue. The report also provided some recommendations for potential solutions.

The report concluded that the overall parking areas north of Surfside Drive were utilized to a higher degree than parking areas south of Surfside Drive and that visitors to the Town during the months of July and August created the greatest requirement for parking. Based on projected population growth for the area, the report also

concluded that, although the Surfside Pier and 13th Avenue South parking lots would be at capacity in several years, the Town had adequate beach access parking to satisfy demand for the next ten years.

In addition, that report noted that opportunities to develop, improve and expand golf cart parking should be a priority. The report also stated that the Town of Surfside Beach should provide safe bikeways and bike storage, as well as participate in planning the route of the East Coast Greenway system thru the Town. Providing safe sidewalks and pedestrian crossings along Ocean Boulevard was also recommended as a priority.

The previous Plan included six (6) general recommendations:

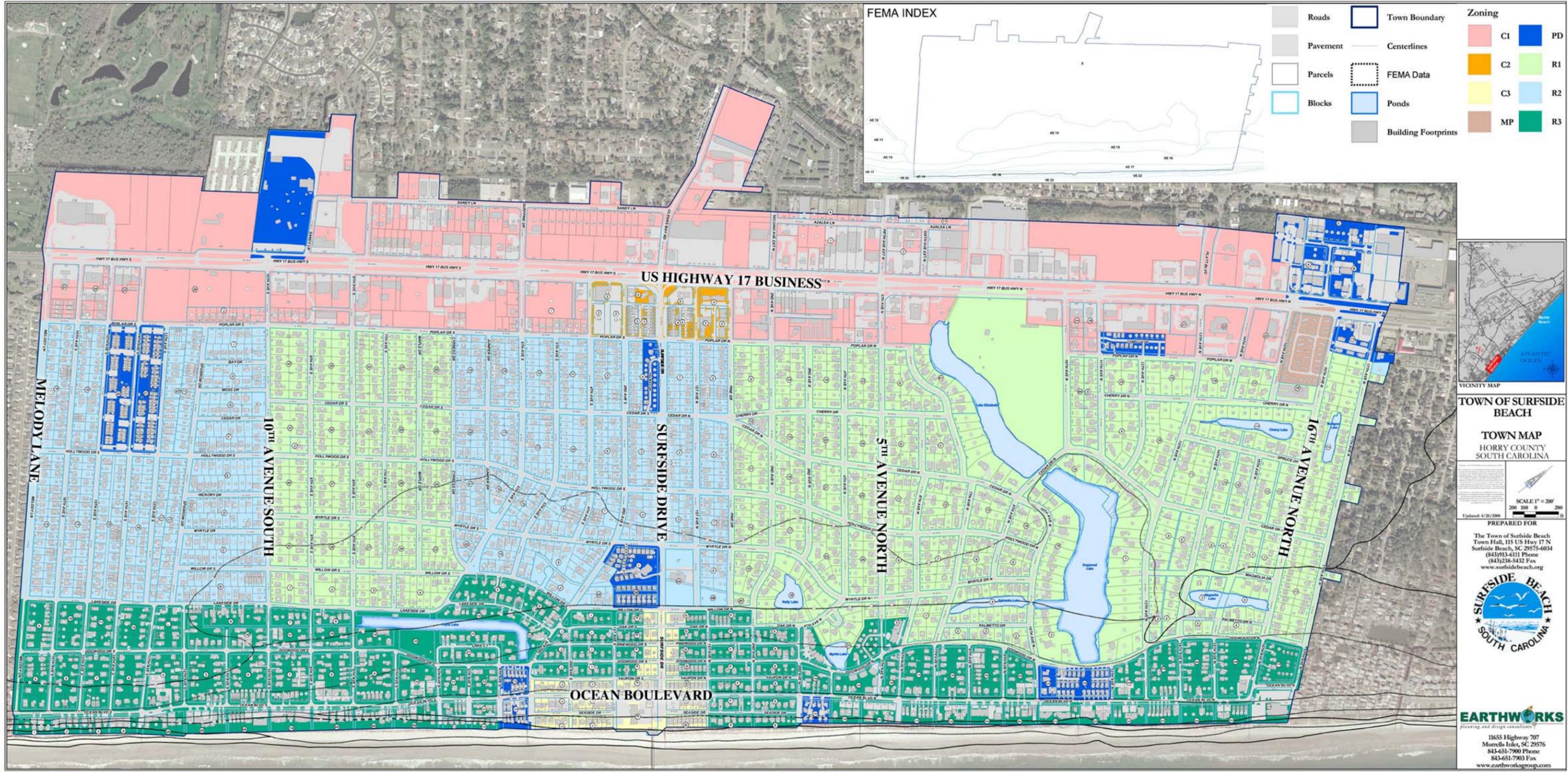
- Increase parking spaces;
- Use existing parking more efficiently;
- Respond to impacts of parking spillover;
- Address variable demand;
- Reduce parking demand; and
- Improve the design of existing facilities.

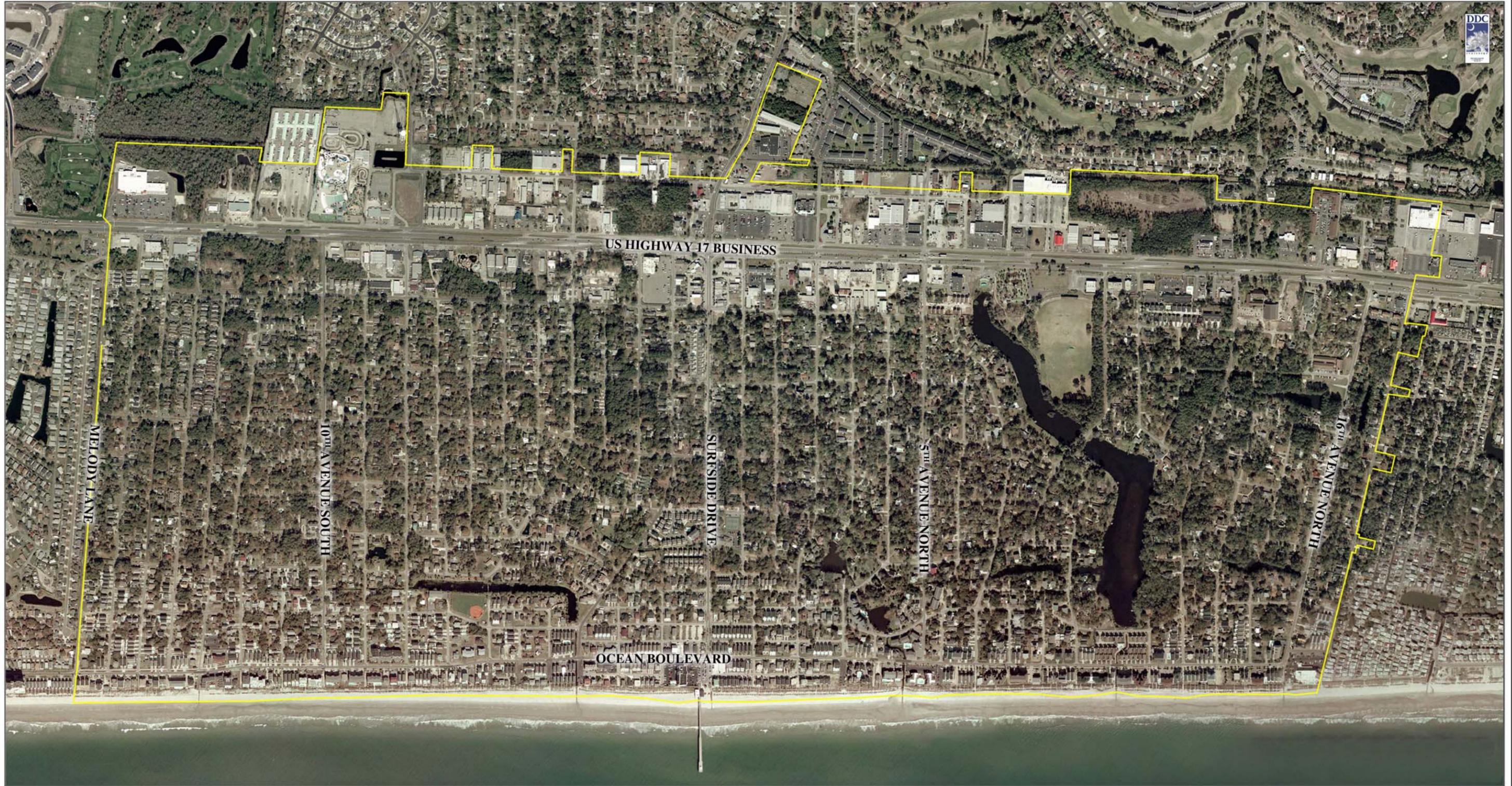
1.3 Key Findings

After being retained to develop a comprehensive parking plan, members of DDC and LandArt staff initiated work in our effort to determine the most feasible program for the Town of Surfside Beach to implement in order to gain control over parking within the Town.

Below is a summary of Key Findings within the report, including an outline of the proposed Course of Action steps towards undertaking the Town's parking problem, listed in order that they are generally recommended for implementation.

- The Town is a dynamic area made up of full-time residents and seasonal visitors who employ the use of golf carts, bicycles and sidewalks, as well as vehicles, to access the beach. Therefore, the Town should endeavor to provide safe and efficient routes for all modes of transportation. (*Section 2.4*)





KEY	
	SURFSIDE BEACH TOWN LIMITS

- Currently, the Town utilizes several measures for controlling parking. These include issuing tickets to parking offenders, installing signage and providing designated metered parking. The Town should continue to utilize these measures while implementing the recommended Course of Action.

1.4 Recommended Course of Action

PHASE ONE

Phase One of this Plan includes steps that the Town should take to utilize and improve upon existing resources. Phase One should be completed within a timeframe between the years 2010-2011.

- Step One of this Phase is for the Town to begin utilizing “boots” or initiate towing for illegally parked cars rather than issuing tickets. Towing sends a strong message to owners of illegally parked cars and will generate more revenue for the Town. *(Section 5.2.3)*
- Step Two of this Phase is for the Town to install parking control methods such as bollards, street trees and curb and gutter. Parking control methods will help to control parking and improve the overall streetscape. *(Section 5.3.2)*
- Step Three of this Phase is for the Town to improve and maintain existing metered parking to ensure that it is utilized to the fullest extent. *(Section 5.2.2)*
- Step Four of this Phase is for the Town to simplify and unify all signage within the Town limits. Developing a cohesive, positive signage system will help to clearly display important information and guide vehicles to metered parking areas. *(Section 4.5)*
- Step Five of this Phase is for the Town to complete and implement the parking lot and park plan at the corner of Surfside Drive and Poplar Drive. *(Section 5.2.5)*
- Step Six of this Phase is for the Town to provide additional parking surrounding the library and amenity area at the corner of Surfside Drive and Myrtle Drive. *(Section 5.2.6, Section 5.3.4-2)*

PHASE TWO

Phase Two of this Plan includes steps that the Town should take to improve vehicular and pedestrian circulation throughout the Town. Phase Two should be completed within a timeframe between the years 2011-2015.

- Step One of this Phase is for the Town to designate major thru-roads, as identified in this report, by employing the use of hardscape and landscape materials. *(Section 4.2)*
- Step Two of this Phase is for the Town to initiate the Sidewalk Implementation Plan. Installing sidewalks along roadways creates a safer environment for pedestrians and encourages the use of transportation forms other than vehicles. *(Section 5.3.1)*

PHASE THREE

Phase Three of this Plan includes steps that the Town should take to utilize available land owned by the Town and suggested agreements that should be made between the Town and local property owners. Phase Three should be completed within an applicable timeframe.

- Step One of this Phase is for the Town to institute a 40/60 split option with owners of vacant oceanfront property. This agreement would allow the Town to provide much needed parking and would generate income for the Town and the property owner.
- Step Two of this Phase is for the Town to negotiate with the owner of the River City Café parking lot to allow the Town to utilize the lot for beach-goers. *(Section 5.3.4-3)*
- Step Three of this Phase is for the Town to provide additional parking in the area surrounding the Dogwood Drive ball field. *(Section 5.3.4-1)*
- Step Four of this Phase is for the Town to provide additional parking at the intersection of Dogwood Drive and 3rd Avenue South. *(Section 5.3.4-4)*

PHASE FOUR

Phase Four of this Plan includes steps that the Town should take to develop a shuttle service from the Legends in Concert parking lot to the Surfside Pier. A shuttle system is an efficient method to transport beach-goers from a parking area to a beach access point, while reducing vehicular traffic congestion. (*Section 5.3.3*) Phase Four should be completed after the completion of Phase Three.

PHASE FIVE

Phase Five of this Plan includes steps that the Town should take to purchase available properties to convert into additional parking areas. These properties have been provided to the Town Administrator and Town Council members under separate cover in an “Executive Session” of Town Council. Phase Five should be completed in the future, as needed and as funds become available.

PHASE SIX

Phase Six of this Plan includes steps that the Town should take to construct a parking garage, including associated retail space on the bottom floor. A parking garage with retail façade would provide the most additional parking within the smallest amount of land space, and would help to enhance the existing streetscape. (*Section 5.3.7*) Phase Six should be completed in the future, to meet growing demand for beach parking.

2. EXISTING CONDITIONS

2.1 Introduction

One objective of the DDC team was to develop an understanding of the history and dynamics of the Town of Surfside Beach as they relate to parking. This section of the report will provide a summary of our understanding of the Town's history as it relates to existing transportation infrastructure, how residents and visitors utilize the roadways and parking areas and where improvements may be made.

2.2 Location and History

The Town of Surfside Beach is located just south of the City of Myrtle Beach in Horry County, South Carolina and makes up a part of what is referred to as the "Grand Strand" (*Exhibit 2.2-1 Location Map*). The Town was first incorporated in 1964 and, according to incorporation documents, included 886 residents at that time. The Town has since grown to over 4,000 residents and is proudly known as "The Family Beach". Surfside Beach encompasses two miles of beach frontage and is an active residential community as well as a popular vacation destination. The Town includes a downtown area, a variety of shops and restaurants, a centrally-located fishing pier and a mix of single and multi-family beach houses, as well as some hotels and small condominium complexes. New commercial development and economic growth along the US Highway 17 Business corridor has increased demand for more efficient transportation, as well as access to the beach. In a pro-active move to control the destiny of Surfside Beach, the Town recently purchased the pier and associated property. (*Image 2.2-1*)



Image 2.2-1 Existing Surfside Pier

2.3 Study Area

For the purpose of this analysis, the study area includes that part of the Town of Surfside Beach which lies east of US Highway 17 Business, from 17th Avenue North, located just south of the limits of Ocean Lakes Campground, to Melody Lane, located just north of the Tupelo Bay Golf Course and the Rose Real Estate office (*Exhibit 2.3-1 Study Area*). Following a relatively intense ground and aerial reconnaissance, several specific areas within the study area were focused on to a higher degree, including the area along and surrounding Ocean Boulevard and including associated existing parking



Image 2.3-1 Existing Surfside Pier

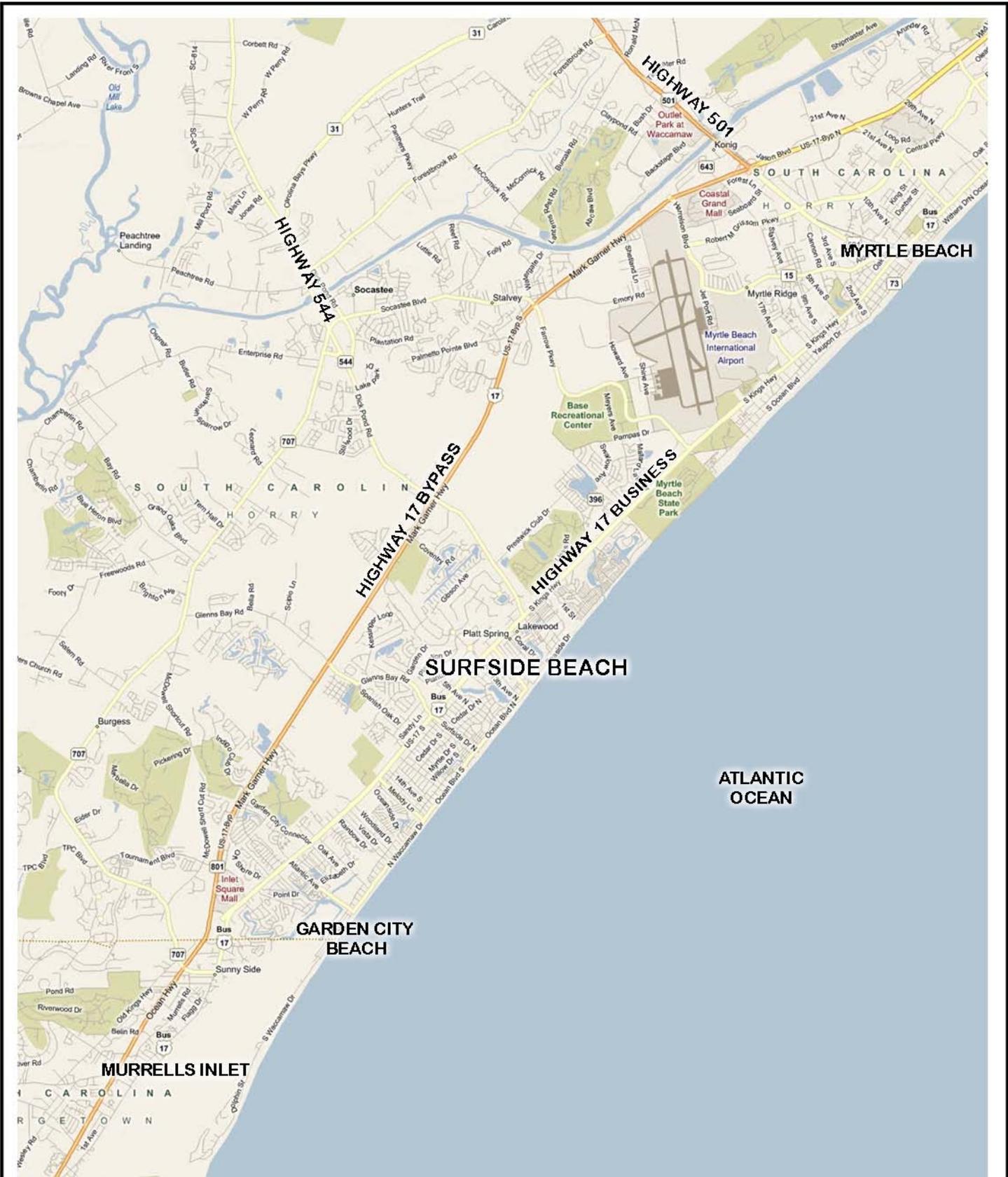
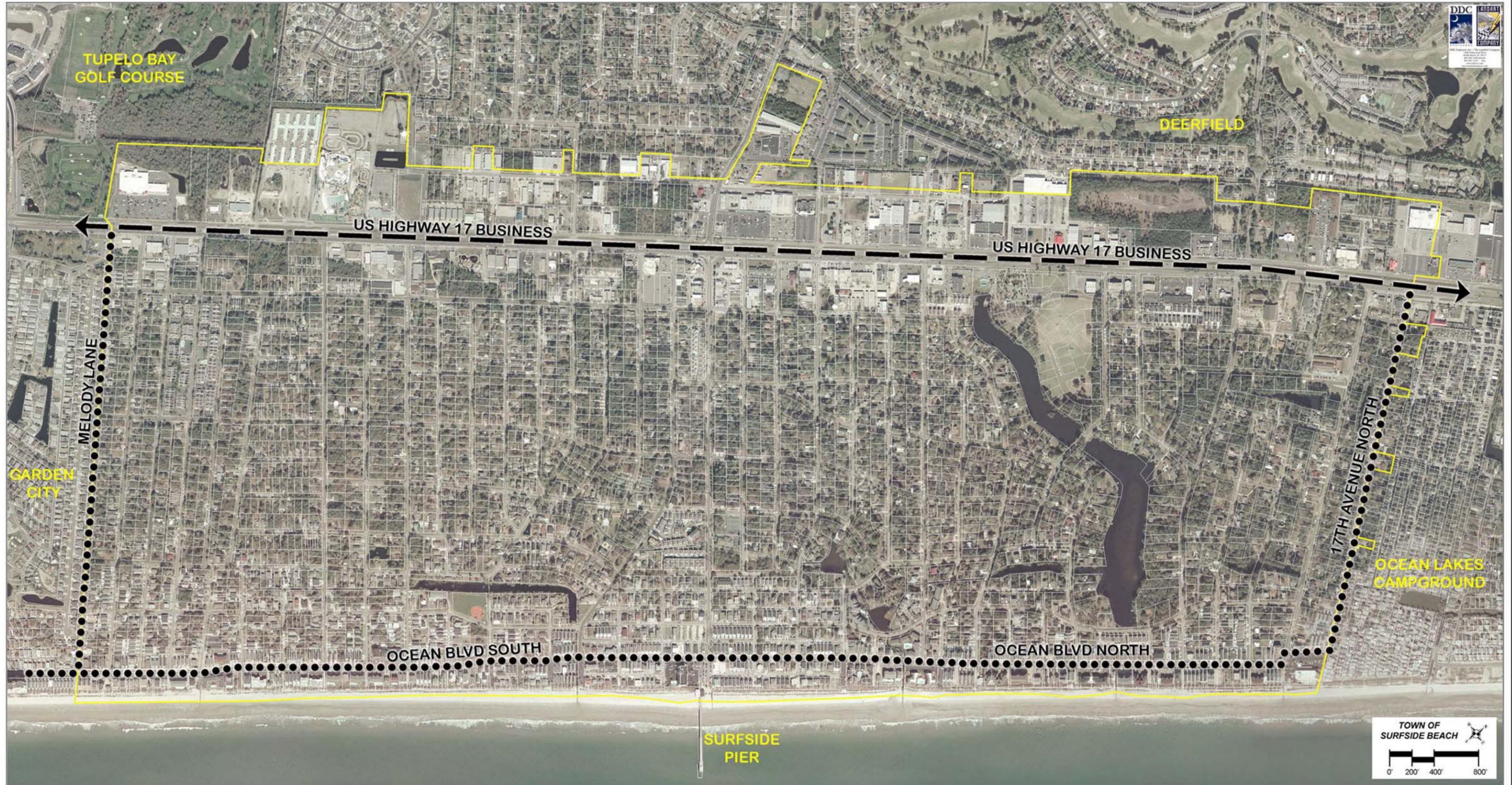


EXHIBIT 2.2-1 LOCATION MAP

TOWN OF SURFSIDE BEACH, SOUTH CAROLINA
 COMPREHENSIVE PARKING STUDY



lots, pedestrian routes and beach access points, golf cart and bike routes. In addition the area along and surrounding Surfside Drive received more comprehensive review. (Image 2.3-1)

2.4 Analysis

The Town of Surfside Beach is a relatively dynamic beach town in which residents and visitors employ the use of vehicles, golf carts, bicycles and pedestrian sidewalks to get from place to place. Therefore, this study includes an analysis of all four modes of transportation in order to better understand the Town's current situation and to suggest improvements.

1. Vehicular Traffic

The major north-south route that serves as an access point for almost all roads within the Town of Surfside Beach is US Highway 17 Business. (Image 2.4.1-1). This highway runs parallel to the Atlantic Ocean and connects Surfside Beach with the City of Myrtle Beach to the north and Murrells Inlet to the south. Ocean Boulevard is also a major north-south route that runs east of US Highway 17 Business and alongside the Atlantic Ocean. Several thru-roads stretch from US Highway 17 Business to Ocean Boulevard, including 17th Avenue North, 16th Avenue North, 13th Avenue North, 5th Avenue North, 2nd Avenue North, Surfside Drive, 3rd Avenue South, 8th Avenue South, 10th Avenue South, 14th Avenue South, 15th Avenue South, 16th Avenue South and Melody Lane. Traffic studies indicate that Surfside Drive and Melody Lane receive the most vehicular traffic by a large percentage.



Image 2.4.1-1 US Highway 17 Business

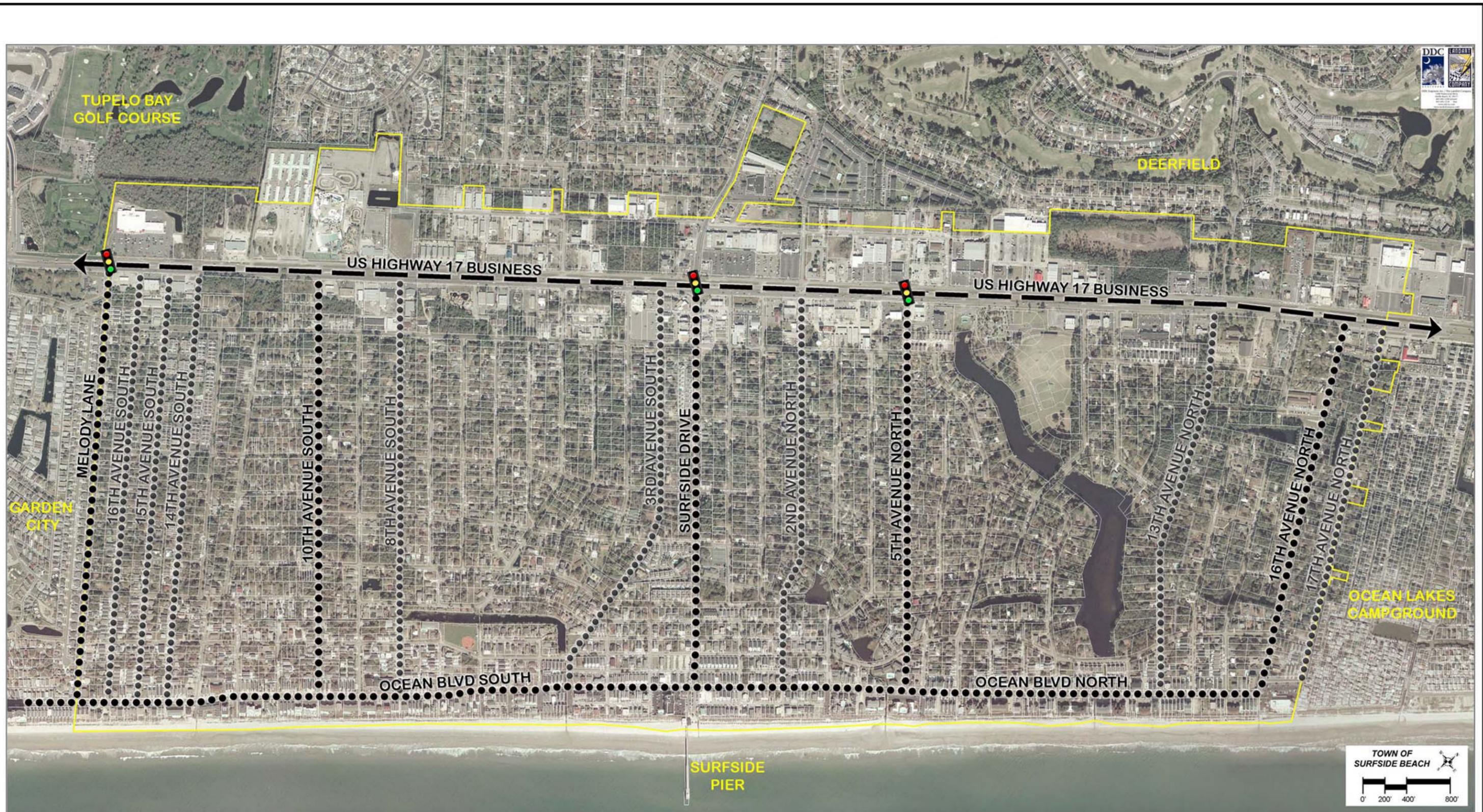
In an effort to determine methods to alleviate congestion on these two roads and to encourage a more even distribution of traffic flow in and out of the Town, five (5) major thru-roads have been identified: (Exhibit 2.4.1-1 Existing Thru Roads).

16th Avenue North (Image 2.4.1-2)

- Owned by State of South Carolina;
- No traffic light at intersection with 17 Business;
- Right-of-way varies between 60'-70';
- Beach access and existing sidewalk; and
- Existing sand golf cart parking.



Image 2.4.1-2 16th Avenue North



KEY	
● ● ●	MAJOR THRU ROAD
● ● ●	THRU ROAD

5th Avenue North (Image 2.4.1-3)

- Owned by State of South Carolina;
- Traffic light at intersection with US Highway 17 Business;
- Right-of-way varies between 55'-60'; and
- No beach access or existing sidewalk.



Image 2.4.1-3 5th Avenue North

Surfside Drive (Image 2.4.1-4)

- Owned by Town of Surfside Beach;
- Traffic light at intersection with US Highway 17 Business;
- Right-of-way varies between 60'-105';
- Beach and pier access and existing sidewalk; and
- Existing vehicular and golf cart parking (asphalt).



Image 2.4.1-4 Surfside Drive

10th Avenue South (Image 2.4.1-5)

- Owned by State of South Carolina, transferred ownership to Town in 2010;
- No traffic light at intersection with US Highway 17 Business;
- Right-of-way varies between 40'-45';
- Beach access and existing sidewalk; and
- Existing paver golf cart parking.



Image 2.4.1-5 10th Avenue South

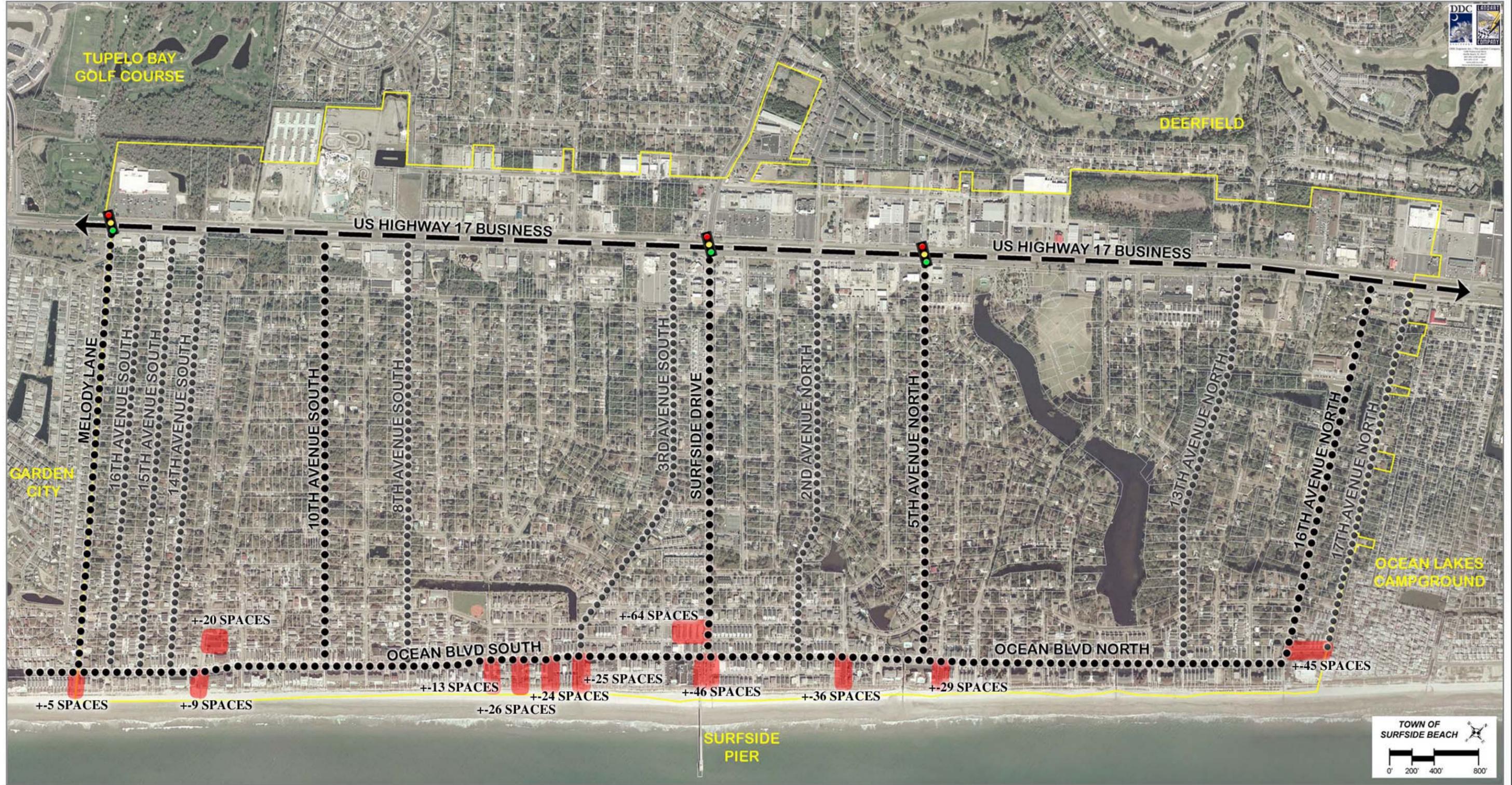
Melody Lane (*Image 2.4.1-6*)

- Owned by State of South Carolina, transferred ownership to Town in 2010;
- Traffic light at intersection with US Highway 17 Business;
- Right-of-way of 75’;
- Beach access and no existing sidewalk; and
- Existing asphalt vehicular and golf cart parking.



Image 2.4.1-6 Melody Lane

Metered vehicular parking is available at several street-end beach access points, as well as in several off-street parking lots located near or adjacent to Ocean Boulevard (*Exhibit 2.4.1-2 Existing Metered Parking*). Although these established municipal parking spaces are available, people typically park alongside roads and in residential areas generally east of Dogwood Drive. The Town of Surfside Beach is addressing this unwanted parking by issuing tickets and erecting “No Parking” signs; however, it continues to be a serious problem especially along Ocean Boulevard, Melody Lane, in areas surrounding the Surfside Pier and in residential areas close to Ocean Boulevard. (*Chart 2.4.1-1 Existing Road and Metered Parking*)



KEY	
	EXISTING METERED PARKING

Chart 2.4.1-1 Existing Road and Metered Parking

STREET NAME	OWNERSHIP	METERED PARKING
MELODY LANE	STATE (TOWN TAKEOVER IN 2010)	5 (1 HANDICAPPED)
16TH AVENUE SOUTH	TOWN	-
15TH AVENUE SOUTH	TOWN	-
14TH AVENUE SOUTH	TOWN	-
13TH AVENUE SOUTH	STATE (TOWN TAKEOVER IN 2010)	20 (2 HANDICAPPED)
13TH AVENUE SOUTH	STATE (TOWN TAKEOVER IN 2010)	9 (0 HANDICAPPED)
12TH AVENUE SOUTH	TOWN	-
11TH AVENUE SOUTH	TOWN	-
10TH AVENUE SOUTH	STATE (TOWN TAKEOVER IN 2010)	-
9TH AVENUE SOUTH	TOWN	-
8TH AVENUE SOUTH	TOWN	-
7TH AVENUE SOUTH	TOWN	-
6TH AVENUE SOUTH	TOWN	13 (0 HANDICAPPED)
5TH AVENUE SOUTH	TOWN	26 (0 HANDICAPPED)
4TH AVENUE SOUTH	STATE (TOWN TAKEOVER IN 2010)	24 (0 HANDICAPPED)
3RD AVENUE SOUTH	STATE (TOWN TAKEOVER IN 2010)	25 (2 HANDICAPPED)
2ND AVENUE SOUTH	TOWN	-
1ST AVENUE SOUTH	TOWN	-
YAUPON DRIVE	TOWN	64 (4 HANDICAPPED)
SURFSIDE DRIVE	TOWN	46 (3 HANDICAPPED)
1ST AVENUE NORTH	TOWN	-
2ND AVENUE NORTH	TOWN	-
3RD AVENUE NORTH	TOWN	36 (2 HANDICAPPED)
4TH AVENUE NORTH	TOWN	-
5TH AVENUE NORTH	STATE	-
6TH AVENUE NORTH	TOWN	29 (0 HANDICAPPED)
7TH AVENUE NORTH	TOWN	-
8TH AVENUE NORTH	TOWN	-
9TH AVENUE NORTH	TOWN	-
10TH AVENUE NORTH	STATE	-
12TH AVENUE NORTH	TOWN	-
13TH AVENUE NORTH	TOWN (FROM CEDAR-OCEAN)	-
14TH AVENUE NORTH	TOWN	-
15TH AVENUE NORTH	STATE (EXCEPT SPRUCE-MAGNOLIA)	-
16TH AVENUE NORTH	STATE	-
OCEAN BOULEVARD	TOWN	45 (1 HANDICAPPED)
17TH AVENUE NORTH	STATE	-
	TOTAL	342 (15 HANDICAPPED)

2. Golf Carts

There are currently no designated golf cart routes or lanes within the limits of the Town of Surfside Beach. Currently, golf carts share the roads with vehicles, often slowing traffic down and creating congestion at peak times of the day. In addition, golf carts often share the sidewalks with pedestrians, causing risk to other users. Golf carts also utilize “drive thru” areas such as residential front yards, private property and parking lots. (Image 2.4.2-1)



Image 2.4.2-1 Golf Cart User

According to the information obtained from the Town, designated golf cart parking is available at every street-end beach access with the exception of 13th Avenue South, 6th Avenue South, 5th Avenue South, 4th Avenue South, 1st Avenue South, 6th Avenue North, 7th Avenue North, 13th Avenue North and 17th Avenue North (Chart 2.4.2-1 Existing Golf Cart Parking). The surface material at most street ends is sand; however, the Town has provided improvements such as pervious pavers, designated golf cart parking, landscaping and signage at several street-ends. (Image 2.4.2-2)



Image 2.4.2-2 Available Golf Cart Parking

Chart 2.4.2-1 Existing Golf Cart Parking

STREET NAME	CART PARKING	MATERIAL
MELODY LANE	2	ASPHALT
16TH AVENUE SOUTH	3	SAND
15TH AVENUE SOUTH	3	SAND
14TH AVENUE SOUTH	2	SAND
13TH AVENUE SOUTH - LOT	-	ASPHALT
13TH AVENUE SOUTH	-	ASPHALT
12TH AVENUE SOUTH	6	SAND
11TH AVENUE SOUTH	6	SAND
10TH AVENUE SOUTH	4	PAVERS
9TH AVENUE SOUTH	6	SAND
8TH AVENUE SOUTH	6	SAND
7TH AVENUE SOUTH	4	PAVERS
6TH AVENUE SOUTH	-	ASPHALT
5TH AVENUE SOUTH	-	ASPHALT
4TH AVENUE SOUTH	-	ASPHALT
3RD AVENUE SOUTH	2	ASPHALT
2ND AVENUE SOUTH	6	PAVERS
1ST AVENUE SOUTH	-	ASPHALT
YAUPON DRIVE - LOT	12	PAVERS
SURFSIDE DRIVE - LOT	2	ASPHALT
1ST AVENUE NORTH	6	PAVERS
2ND AVENUE NORTH	6	PAVERS
3RD AVENUE NORTH	2	ASPHALT
4TH AVENUE NORTH	8	SAND
5TH AVENUE NORTH	N/A	N/A
6TH AVENUE NORTH	-	ASPHALT
7TH AVENUE NORTH	-	SAND
8TH AVENUE NORTH	10	SAND
9TH AVENUE NORTH	6	SAND
10TH AVENUE NORTH	8	SAND
12TH AVENUE NORTH	8	SAND
13TH AVENUE NORTH	-	SAND
14TH AVENUE NORTH	8	SAND
15TH AVENUE NORTH	6	SAND
16TH AVENUE NORTH	6	SAND
OCEAN BOULEVARD - LOT	-	ASPHALT
17TH AVENUE NORTH	-	SAND
TOTAL	138	

3. **Bicycling**

Many residents and visitors use bicycles as an alternative form of transportation when making a short trip to Town Hall, the library, pier and park areas or for an evening outing. Currently there are no designated bike routes or lanes in the Town of Surfside Beach. The Town has provided bike racks in front of Town Hall and at several street-end beach accesses. The Town should continue to accommodate bicycles to encourage their use as an alternative to motor vehicles. Although surrounding municipalities, such as the City of Myrtle Beach and Murrells Inlet, have designated bicycle routes, the Town of Surfside Beach does not. (Image 2.4.3-1)



Image 2.4.3-1 Bicycle User

4. **Pedestrian**

The Town of Surfside Beach currently has the beginning of an interconnected sidewalk system in place along several roads adjacent to the beach. A sidewalk has been installed on both sides of Surfside Drive from US Highway 17 Business to Ocean Boulevard, and is separated from vehicular traffic by a planted landscape strip. There are also existing sidewalks on the eastern side of Ocean Boulevard that run from 17th Avenue North to Melody Lane and on the western side of Ocean Boulevard that run from 10th Avenue South to Melody Lane and from 2nd Avenue South to 1st Avenue North (Exhibit 2.4.4-1 Existing Sidewalks). Several other Town roads have sidewalks; however, the majority of roads in the areas adjacent to Ocean Boulevard do not.



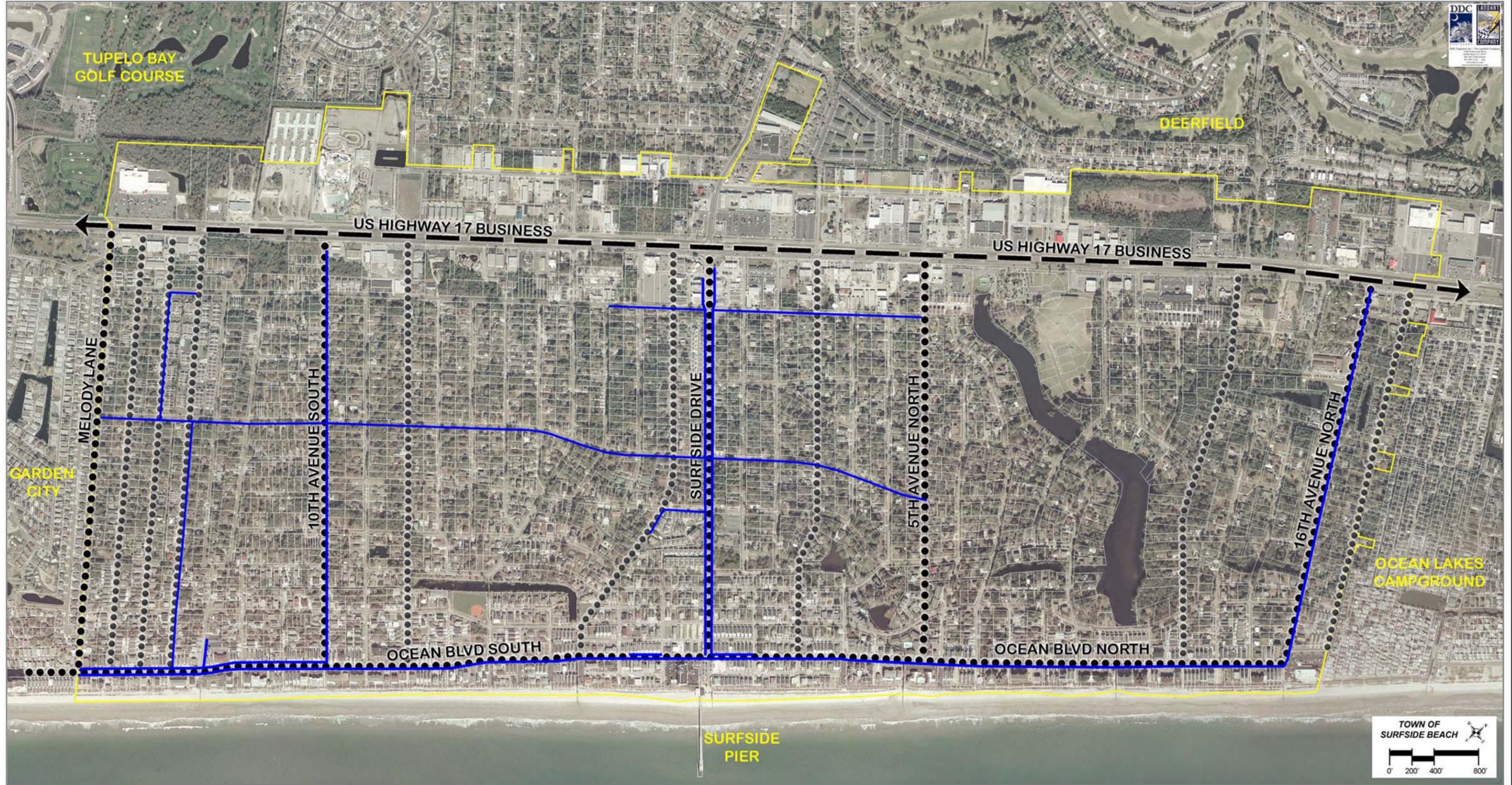
Image 2.4.4-1 Sidewalk User

The Town of Surfside Beach offers beach access at every street-end, with the exception of 5th Avenue North, for a total of thirty-three (33) access points. Although beach-goers generally walk on the side of the road, creating a potentially unsafe situation, the Town has painted crosswalks on Ocean Boulevard at various street ends to assist motorists in being aware of pedestrians and to encourage safe street crossing. (Image 2.4.4-2)



Image 2.4.4-2 Pedestrian Crosswalk (Ocean Boulevard)

The majority of roads in the Town of Surfside Beach do not have a curb and gutter system in place. Asphalt roads are “crowned” to drain water to either side, where a drainage swale, ditch or pipe collects and moves the water. There are areas where the drainage



KEY	
	EXISTING SIDEWALK

has been altered so that access to buildings, driveways and sidewalks can be possible; however, many areas continue to have open drainage which prevents people from parking on the side of the road. Unfortunately this also inhibits the implementation of sidewalks and street trees. (Image 2.4.4-3)



Image 2.4.4-3 Existing Drainage Swale

2.5 Section Summary

The Town of Surfside Beach has grown significantly from the time of its incorporation and is currently lacking adequate infrastructure and public spaces needed to meet the Town's growing needs. The Town's main attraction is the beach and, recognizing the importance of this, the Town has provided beach accesses at every street-end, with the exception of 5th Avenue North, in an effort to meet the demand. Residents and visitors also have the ability to utilize several types of transportation including vehicles, golf carts, bicycles and pedestrian sidewalks. There are thirteen (13) roads that currently run from US Highway 17 Business to Ocean Boulevard. Five (5) of these roads have been identified as major thru roads and should be utilized to help filter visitors through the Town and to help relieve traffic congestion on Surfside Drive. There is metered parking available throughout the Town and especially within close proximity to the beach. There are no designated golf cart lanes; however golf cart parking is available. The Town does not have designated bicycle lanes and could benefit from additional sidewalks and / or multi-purpose paths alongside roads. Although the Town currently has good transportation framework, improvements need to be implemented so that each type of transportation can be safely utilized to the full benefit of the Town, its residents and visitors.

3. EXISTING DATA

3.1 Introduction

Currently, the Town of Surfside Beach provides metered parking at a rate of \$1.00/hour. This rate is significantly higher when compared to \$0.50/hour in 2003, according to the previous Study. Metering occurs at nine (9) of the Town's thirty-three (33) street-end beach accesses and at three (3) off-street parking lot locations near Ocean Boulevard. This section of the report will identify where public parking is available, utilization of public parking at peak periods, common parking violations and the effect the current parking situation has on the Town.

3.2 Supply and Demand

Members of DDC and LandArt staff studied how and when public parking lots were utilized through several site visits during the month of July, 2009. Parking is well dispersed throughout the town and easily accessible to vehicles. Overall, there are forty-six (46) available spaces in one (1) lot at the end of Surfside Drive, one hundred eighty-six (186) available spaces in eight (8) parking areas south of Surfside Drive and one hundred and ten (110) available spaces in three (3) parking areas north of Surfside Drive.

The lot located at the end of Surfside Drive has the highest usage rate of all available parking areas. This parking lot stays busy for a number of reasons. These include: central location within close proximity to the pier and several restaurants; location at the end of a major thru-road, and that there is little opportunity in this area for "free" public parking. (Image 3.2-1)

Located just south of Surfside Drive, the Yaupon Drive parking lot was recently constructed utilizing pavers. This attractive addition provides sixty-four (64) metered spaces. It is located approximately 550' from the beach. This lot offers beach-goers a direct, safe route to the pier and restaurant area, as well as convenient vehicular access to and from Surfside Drive. Although pedestrians must cross Ocean Boulevard to access the beach, this parking lot was observed to be well-utilized during the peak beach season. (Image 3.2-2)



Image 3.2-1 Surfside Drive Parking Lot



Image 3.2-2 Yaupon Drive Parking Lot

Although the Surfside Pier area currently has over one hundred (100) available parking spaces, research indicates that it would benefit the Town to provide more parking in this area.

The parking lot furthest north of Surfside Drive, between 17th Avenue North and 16th Avenue North, includes forty-five (45) metered spaces and is located approximately 300' from the beach. Beach-goers can access the beach by way of the 17th Avenue North or 16th Avenue North beach access. This parking area is currently under-utilized. It was observed that only 25-33% of the spaces were being used during peak periods. Both the 6th Avenue North and 3rd Avenue North beach access parking lots were observed to be well-utilized. Despite available metered parking, vehicles were parallel parked along Ocean Boulevard, between 16th Avenue North and 3rd Avenue North, in un-metered, un-marked locations. This observation leads to the conclusion that more metered parking is needed in this area and that steps should be taken to discourage unwanted parking so that the Town can take advantage of potential revenue. (Image 3.2-3)



Image 3.2-3 16th - 17th Avenues North Parking Lot

Located south of Surfside Drive, the 3rd Avenue South, 4th Avenue South, 5th Avenue South and 6th Avenue South beach access parking lots were observed to be well-utilized. These parking lots include a total of eighty-eight (88) metered spaces. However, vehicles were parallel parked along Ocean Boulevard, in front of the ball field located on Dogwood Drive and along other residential roads in un-metered, un-marked locations. (Image 3.2-4)



Image 3.2-4 Street-End Parking Lot

The two 13th Avenue South parking lots were observed to be well-utilized; however, despite a very large "Parking" sign on Ocean Boulevard, it was observed that visitors were more likely to parallel park along a residential road than in the 13th Avenue South lot located approximately 450' away from the beach. The Melody Lane beach access parking lot was also observed to be well-utilized. Vehicles were parked along Melody Lane in un-metered, un-marked locations as well. As was concluded in the area north of Surfside Drive, the Town would benefit from more metered parking south of Surfside Drive, coupled with a program to prevent parking on the street rights-of-way. (Image 3.2-5)



Image 3.2-5 13th Avenue South Parking Lot

3.3 Parking Violations

The Town of Surfside Beach issued a total of seven hundred and sixty (760) parking violations during the months of May, June and July 2009, according to Town records. Of those violations issued, 78% (or 592) were for violations associated with parking meters. The remaining tickets issued were for violations such as parking in "no parking" zones or on a sidewalk, parking too close to an intersection or stop sign and for obstructing a vehicular right-of-way (Chart 3.3-1).

Chart 3.3-1 Parking Violations

PARKING VIOLATIONS					
MONTH	METERED	*OTHER			
May-09	177	13			
Jun-09	209	58			
Jul-09	206	97			
*BREAKDOWN OF "OTHER" CATEGORY					
MONTH	POSTED "NO PARKING"	TOO CLOSE TO INTERSECTION	OBSTRUCTING R.O.W.	TOO CLOSE TO STOP SIGN	PARKED ON SIDEWALK
May-09	2	2	7	-	2
Jun-09	8	20	20	2	8
Jul-09	35	34	25	-	3

3.4 Parking Revenue

According to Town records, a total of \$88,359 was collected from parking meters during the months of May, June and July 2009, with July being the busiest month of the three. During this time period, an additional total of \$19,506 was collected as a result of parking violations, bringing total revenues associated with parking to \$107,865.00 for the three (3) summer months of 2009 (Chart 3.4-1).

Chart 3.4-1 Parking Revenue

PARKING REVENUE			
MONTH	METERS (\$)	TICKETS (\$)	
May-09	\$ 13,693.00	\$ 3,831.00	(134 PROCESSED)
Jun-09	\$ 29,834.00	\$ 7,000.00	(210 PROCESSED)
Jul-09	\$ 44,832.00	\$ 8,675.00	(240 PROCESSED)
TOTAL	\$ 88,359.00	\$ 19,506.00	(584 PROCESSED)
NOTE: METERED PARKING IS \$1.00/HOUR			

3.5 Section Summary

Currently, metered parking is an excellent source of revenue and is widely available throughout the Town of Surfside Beach. However, it is apparent that many people continue to park in prohibited areas, often when adequate metered spaces remain available. The Town would benefit from providing additional metered parking in the south, north and pier areas, keeping meter prices at a reasonable rate and developing a program to further restrict illegal parking while continuing to issue tickets and tow vehicles for parking violations.

4. STREETScape AND SIGNAGE

4.1 Introduction

The Town of Surfside Beach has a variety of dynamic commercial, residential and recreational developments; however, the Town is lacking overall interconnection and a common design theme. This section of the report will identify ways that the Town can create a “signature look” which will provide a more cohesive, functional community.

4.2 Road Characteristics

The Town of Surfside Beach has several thru-roads that extend from US Highway 17 Business to Ocean Boulevard. Five (5) “major thru-roads” have been previously identified. A clear set of design characteristics should be defined to emphasize the significance of these roads, especially from US Highway 17 Business, and to encourage their use. A “major thru-road” should include striped lanes and be at least 22’ wide. No vehicular parking should be allowed on either side of the road so that traffic is able to flow smoothly; driveways should be the only exception to this. An 8’ multi-purpose path or a 5’ sidewalk should be constructed on at least one side of the road and street trees or palmettos should be installed on either side of the road. (Image 4.2-1)



Image 4.2-1 Intersection of Surfside Drive and US Highway 17 Business

Three (3) of the five (5) “major thru-roads” have existing traffic signals at the intersection with US Highway 17 Business. To draw attention to and make it easier for people to access these roads, a traffic signal should be installed at the intersections of US Highway 17 Business with 16th Avenue North and 10th Avenue North, respectively. A complimentary hardscape feature, such as a low wall on either side of the road, should be constructed, along with landscaping at each of the five intersections to help visually establish a hierarchy system. The feature at Surfside Drive should be slightly larger or more elaborate to signify entrance into the center of the Town. Hardscape features will not only help to guide vehicular traffic, but will also play a large part in establishing the Town’s “signature look”. (Image 4.2-2)



Image 4.2-2 Hardscape Feature

4.3 Ocean Boulevard

Ocean Boulevard is owned by the Town of Surfside Beach and is one of the Town's most heavily utilized roads. The Ocean Boulevard right-of-way varies from 75' wide in residential areas at both the south and north ends of the Town, to 40' wide between 3rd Avenue South and 3rd Avenue North in the middle of the Town. Because Ocean Boulevard provides a venue for vehicles, golf carts, bicycles and pedestrians, it is important that the road layout and streetscape are designed for efficiency, functionality and safety. The streetscape theme and quality should also reflect the Town's "signature look" and give residents and visitors a sense of arrival to the beach.

The current Ocean Boulevard 40' wide right-of-way serves both commercial and residential users as well as beach-goers. The majority of the eastern side of the road has a sidewalk and curb and gutter system; however, the western side of the road varies between a sidewalk, wide vehicular travel lanes and parking spaces and does not have curb and gutter. Overall, the current streetscape condition lacks a sense of consistency. As such, it often creates vehicular congestion and unsafe situations for pedestrians. There are several options the Town can implement to help make this area more functional and appealing. (Image 4.3-1, Image 4.3-2)



Image 4.3-1 North Ocean Boulevard 40' R.O.W.

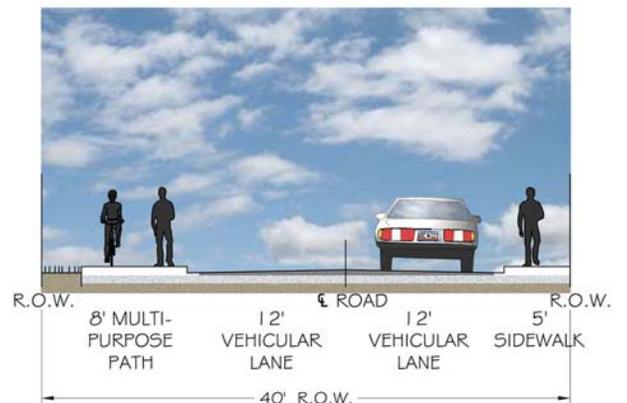


Image 4.3-2 South Ocean Boulevard 40' R.O.W.

Proposed Improvement Options - 40' R.O.W.

1. Elevation 4.3-1

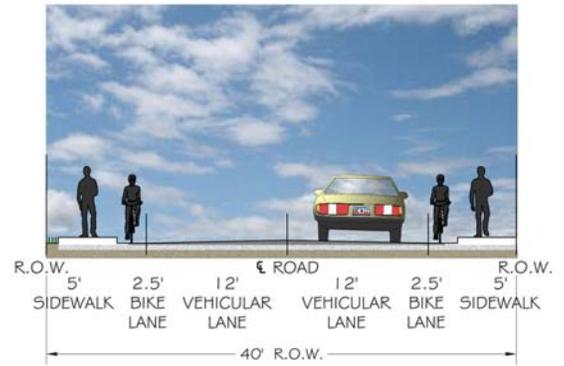
The existing sidewalk and curb and gutter on the eastern side of Ocean Boulevard would remain; existing vehicular lanes would remain; an 8' multi-purpose path would be installed with curb and gutter on the western side of Ocean Boulevard. The Town would be able to use existing sidewalk, curb and gutter and road layout. The multi-purpose path would help separate vehicles from other modes of transportation as well as provide a wide safe bicycle route for the East Coast Greenway.



Elevation 4.3-1 Ocean Boulevard 40' R.O.W.
3rd Avenue South to 3rd Avenue North

2. Elevation 4.3-2

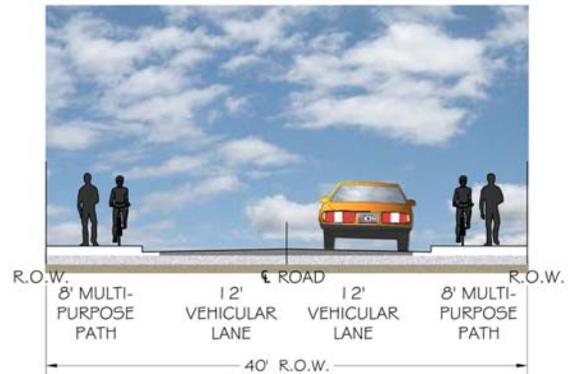
The existing sidewalk and curb and gutter on the eastern side of Ocean Boulevard would remain; existing vehicular lanes would be widened to accommodate two (2) 2.5' wide bike lanes, one (1) in either direction; a 5' wide sidewalk would be installed with curb and gutter on the western side of Ocean Boulevard. The Town would be able to use existing sidewalk and curb and gutter. The designated bike lanes would provide a safer bicycle route for the East Coast Greenway and the sidewalk on the western side of the road would provide additional safe pedestrian space separated from vehicles.



Elevation 4.3-2 Ocean Boulevard 40' R.O.W.

3. Elevation 4.3-3

The existing sidewalk would be expanded to an 8' multi-purpose path with curb and gutter on the eastern side of Ocean Boulevard; vehicular lanes would shift to accommodate; an 8' multi-purpose path would be installed with curb and gutter on the western side of Ocean Boulevard. A multi-purpose path and curb and gutter on either side of the road would define vehicular space from other modes of transportation, provide a wide safe bicycle route for the East Coast Greenway and provide safe pedestrian space.



Elevation 4.3-3 Ocean Boulevard 40' R.O.W.

The current Ocean Boulevard 75' wide right-of-way serves residential users and beach-goers to the greatest extent. The eastern side of Ocean Boulevard has a sidewalk for the entire length of the road. The western side has a sidewalk from Melody Lane to 10th Avenue South. There is no curb and gutter, and both sides of the road are cluttered with driveways and other access points. Currently, vehicles parallel park alongside Ocean Boulevard where they can find space and beach-goers walk alongside the western side of the road until they are able to cross.



Image 4.3-3 North Ocean Boulevard 75' R.O.W.

Because the right-of-way is wide in these areas, the Town has an opportunity to implement streetscape improvements that will provide a safe bicycle route for the East Coast Greenway, help make this area more functional and appealing, and at the same time, eliminate the ability for individuals to utilize the area for parking. There are several potential methods available to implement this plan. (Image 4.3-3, Image 4.3-4)

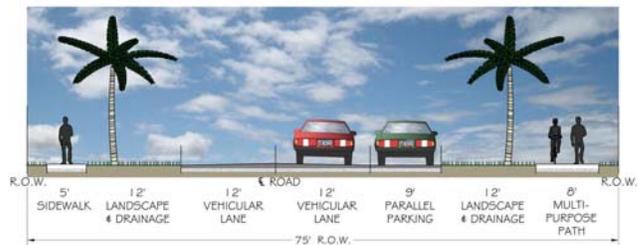


Image 4.3-4 South Ocean Boulevard 75' R.O.W.

Proposed Improvement Options - 75' R.O.W.

1. Elevation 4.3-4

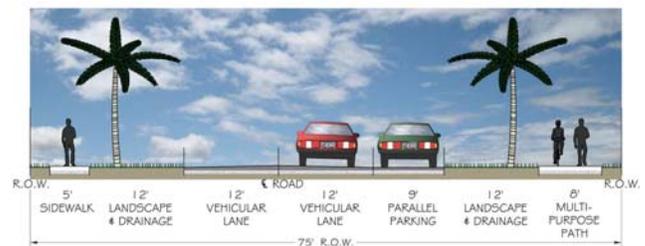
The existing sidewalk on the eastern side of Ocean Boulevard would be expanded to an 8' multi-purpose path. A wide landscape and drainage area would be maintained and 9' wide parallel parking would be allowed on the eastern side of Ocean Boulevard. Vehicular lanes would be shifted to accommodate; a wide landscape and drainage area would be maintained and a 5' wide sidewalk would be installed on the western side of Ocean Boulevard. Designating parallel parking spaces on the eastern side of Ocean Boulevard would allow the Town to generate revenue from parking that is already taking place. This scenario should be used in areas where there is enough space between access points on Ocean Boulevard to accommodate parallel parking, such as the area around 14th Avenue North. A wide landscape and drainage area would separate vehicular and pedestrian traffic as well as maintain the Town's existing drainage system. A multi-purpose path would accommodate more golf carts, bicycles and pedestrians on the busier side of the road, while a 5' wide sidewalk would still allow pedestrian travel on the western side of Ocean Boulevard.



Elevation 4.3-4 Ocean Boulevard 75' R.O.W.

2. Elevation 4.3-5

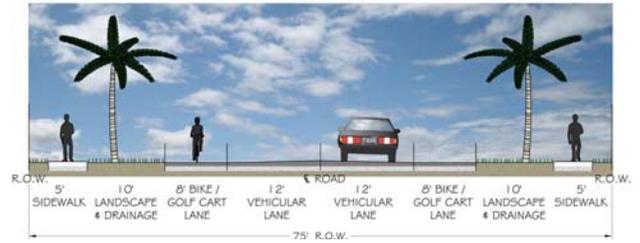
The existing sidewalk on the eastern side of Ocean Boulevard would be expanded to an 8' multi-purpose path; a wide landscape and drainage area would be maintained on either side of the road; vehicular lanes would shift to accommodate; an 8' multi-purpose path would be installed on the western side of Ocean Boulevard. Having multi-purpose paths on either side of the road would allow golf carts, bicycles and pedestrians ample space to travel along Ocean Boulevard without crossing into vehicular traffic. A planted landscape and drainage area would help to prevent unwanted parking along the road as well as maintain the existing drainage system, creating a safer, more functional streetscape.



Elevation 4.3-5 Ocean Boulevard 75' R.O.W.

3. Elevation 4.3-6

The existing sidewalk on the eastern side of Ocean Boulevard would remain; the existing landscape and drainage area would also remain; existing vehicular lanes would be widened to accommodate two 8' wide golf cart and bicycle lanes, one in either direction; a landscape and drainage area would be maintained and a 5' wide sidewalk would be installed on the western side of Ocean Boulevard. Designated lanes alongside the road would allow golf carts and bicycles to travel out of the way of vehicles, while 5' wide sidewalks away from the road would allow pedestrians a safe travel space of their own. A planted landscape and drainage area would also help prevent unwanted parking along the road, protect pedestrians from vehicular traffic and maintain the Town's existing drainage system.



Elevation 4.3-6 Ocean Boulevard 75' R.O.W.

4.4 Downtown Area

The Town of Surfside Beach's "downtown" consists of the area generally between 3rd Avenue South and Pine Drive from US Highway 17 Business to Poplar Drive. This area includes a number of businesses and restaurants, as well as the Town Hall and service buildings. Surfside Drive, one of the busiest roads within the Town limits, traverses through the downtown area. Angled parking is available on either side of the road and adjacent to sidewalks that front the existing building facades. Although the existing sidewalks separate pedestrian from vehicular space, they are not sized appropriately for the area and are in disrepair. Within the downtown area, there are no existing crosswalks on Surfside Drive which, because of the road's one hundred twenty five foot (125') right-of-way and vast expanse of asphalt, creates a hazardous situation for pedestrians. Surfside Drive is lined with well-established shade trees; however, existing utility lines also parallel either side of the road, preventing the trees from reaching a proper mature size and creating a maintenance issue for the Town. (Image 4.4-1, Image 4.4-2) The Town is currently nearing construction completion of its new fire station, located behind the buildings on the north side of Surfside Drive. This project is part of a larger renovation to be carried out in the future in this area, including a new Town Hall complex and additional organized parking.



Image 4.4-1 Existing Downtown Surfside Drive

In an effort to enhance the downtown area and improve the flow of vehicular and pedestrian traffic, the Town should consider an overall streetscape revitalization (*Exhibit 4.4-1 Conceptual Streetscape Plan*), including both hardscape and landscape improvements. The existing sidewalks, planting areas within the roadway, shade trees in front of the buildings and other existing streetscape objects should be removed. The existing utility lines should be either removed or buried underground to allow proper room for shade trees, as well as to improve the quality of the streetscape.

A new sidewalk, ranging in width from ten (10) to fifteen (15) feet should be installed. Angled parking should also be installed; however, a large island should be provided near the middle of the parking spaces and on either side of the road. Surfside Drive should include four eleven foot (11') lanes, two in either direction, between Floral Drive and Poplar Drive and include a five (5) to ten (10) foot raised planted median. Having two vehicular traffic lanes in either direction will help ease congestion along the road as vehicles pull into and back out of parking spaces. A pedestrian crosswalk should be installed to cross Surfside Drive and should be aligned with the parking islands on either side of the road. Handicapped access in both islands and a break in the center median will allow pedestrians to easily and safely cross the road.

Deciduous shade trees should be planted in tree grates along the sidewalk close to the curb. Deciduous trees will provide shade to pedestrians and existing buildings in the spring and summer months, but will lose leaves in the fall and winter, providing sunlight and warmth to pedestrians and buildings during the colder seasons. Evergreen shrubs and groundcovers should be planted in the center median to provide year-round vegetation. Flowering shrubs and perennials should also be used in the center median and other planting areas along the streetscape to provide color.

In the area surrounding Town Hall and the new fire station, parking lots should be designed to compliment the building layout and provide additional parking. Converting vacant property into parking will improve the overall functionality and appearance of this area.



Image 4.4-2 Existing Downtown Surfside Drive



Image 4.4-3 Existing Downtown Surfside Drive

PLAN IS CONCEPTUAL IN NATURE AND SHOULD NOT BE USED AS A CONSTRUCTION DOCUMENT OF ANY KIND. PLAN IS BASED ON CAD INFORMATION SUPPLIED BY THE EARTHWORKS GROUP AND DP3 ARCHITECTS, LTD.



4.5 Signage

The Town of Surfside Beach has been utilizing signage in an effort to direct traffic, restrict parking and direct visitors to metered parking, and make residents and visitors aware of Town policies. However, there is no continuity to the types of signage the Town is currently utilizing, which is creating a cluttered, confusing environment for visitors. Although the Town has many metered parking areas available, these areas are often missed due to poor or confusing signage.

A cohesive signage or “way-finding system” is important to reduce the number of conflicting messages a driver experiences while concentrating on the task of driving. A unified signage system will also assist motorists to draw attention to important information such as the locations of parking areas. In addition, the creation of attractive streetscapes will provide a sense of identity for the Town. The Town would benefit from agreeing upon and creating a unique “signature look” and carrying that design thru with attractive streetscapes and cohesive signage designating vehicular direction, parking, Town policy, beach and park rules, pedestrian crossings and other activities. (Image 4.5-1)



Image 4.5-1 Existing Signage

4.6 Section Summary

Improving the streetscape using both hardscape and plant materials will help the Town of Surfside Beach to establish a “signature look”. At the same time, these enhancements will provide improvements to the Town’s overall parking and transportation problems. Cohesive signage will guide and inform visitors, while adding to the streetscape. It is also important for the Town to establish a hierarchy of roads and identify “major thru-roads” from US Highway 17 Business to Ocean Boulevard. Improvements to Ocean Boulevard will help to reinforce the Town’s “signature look” and create a more useable streetscape that residents and visitors can enjoy.

5. CONCLUSIONS AND RECOMMENDATIONS

5.1 Introduction

The Town of Surfside Beach has already taken several measures to improve the Town's streetscapes, parking and circulation. In an effort to guide the Town in carrying out additional improvements, this section offers support for existing programs, as well as additional suggestions and possible programs that the Town may implement in the future.

5.2 Existing Measures

1. Beautification Advisory Committee

The Town of Surfside Beach has already established a Beautification Advisory Committee, which has organized and made several improvements within the Town limits. The Committee's mission statement is "To enhance the quality of life for Surfside Beach residents, visitors and business owners by providing aesthetically attractive and well designed streetscapes for all public areas including facades, creating a pleasing more beautiful signature look for Surfside Beach." The Committee should continue to raise community awareness through the implementation of projects and fundraisers, recruitment of volunteers and also to continue to seek out State and Federal grants to improve the Town's facade. The Committee should also take the lead role in ensuring a unified effort in which all improvements promote the Town's "signature look". Prioritizing improvements is also important to ensure that money is invested in projects that will most greatly benefit the Town. (Image 5.2.1-1, Image 5.2.1-2)



Image 5.2.1-1 Downtown Facade "Before"



Image 5.2.1-2 Downtown Facade "After"

2. Metered Parking

Currently, all public parking, outside of the Town's downtown area, is metered. Any additional parking, except shuttle parking if implemented, should be metered as well. The Town should continue to keep its parking rates reasonable and update older parking meters with new, more flexible models to ease the process of paying at the meters. The Town should also extend its metered parking hours from 7am to 7pm. A timeframe that includes all summer daylight hours will ensure that the Town benefits from all beach parking. Also important is for the Town to keep parking areas safe, attractive and easily accessible from major roads. For example, people will be more apt to use an available parking lot if the area is well maintained and within a reasonable walking distance to the beach. Vehicular traffic may also flow smoothly when a parking lot has optimal access points. Metered parking has proven to be a good source of income for the Town, as well as an organized alternative to scattered parking alongside residential roadways. (Image 5.2.2-1)



Image 5.2.2-1 Metered Parking Lot

3. Ticketing and Towing

Issuing tickets for parking violations has proven to be somewhat beneficial for the Town given the fact that it is a consistent source of income and that it encourages people to utilize designated metered parking spaces. In addition to issuing tickets, the Town should consider implementation of towing or "booting" illegally parked cars. These methods send a much stronger message to owners of these vehicles and will create more revenue for the Town in the short term. Vehicle owners may elect to pay the ticket and park illegally, but few will illegally park a second time after their vehicle is towed. The Town should continue to consistently enforce the laws against prohibited parking and focus on repeat offenders. (Image 5.2.3-1)



Image 5.2.3-1 Vehicles Parked in Prohibited Area

4. Street-End Improvements

The Town of Surfside Beach has improved many of its street-end beach access points by installing pervious pavers for golf cart parking, as well as installing associated landscaping and signage. Currently, street-end beach accesses at 2nd Avenue North, 1st Avenue North, 2nd Avenue South, 7th Avenue South and 10th Avenue South have been improved. The Town should continue to implement improvements at street-ends that are currently sand, focusing first on 16th Avenue North, the remaining “major thru-road” street-end. Future improvements should be made using the same materials that were used in existing street-ends in order to establish a uniform look for Surfside Beach. (Image 5.2.4-1, Image 5.2.4-2)



Image 5.2.4-1 Sand Street-End



Image 5.2.4-2 Paver Street-End

5. Surfside Drive and Poplar Drive Parking and Park (Exhibit 5.2.5-1)

The Town is currently working to convert a vacant piece of land located at the corner of Surfside Drive and Poplar Drive into a small parking lot and passive park area. The project is still in the planning phase and will require approval prior to construction; however, its completion will provide an additional twenty (20) parking spaces near the downtown area. Plant materials and a meandering sidewalk will provide a pleasant area for residents and visitors to use. The Town should proceed with working towards completing this project, as it will provide additional parking, as well as add aesthetic value along one of the Town’s busiest roads. (Image 5.2.5-1)



Image 5.2.5-1 Corner of Surfside Drive and Poplar Drive

6. Library Expansion and Amenity Area

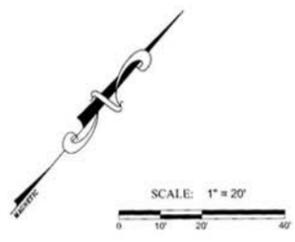
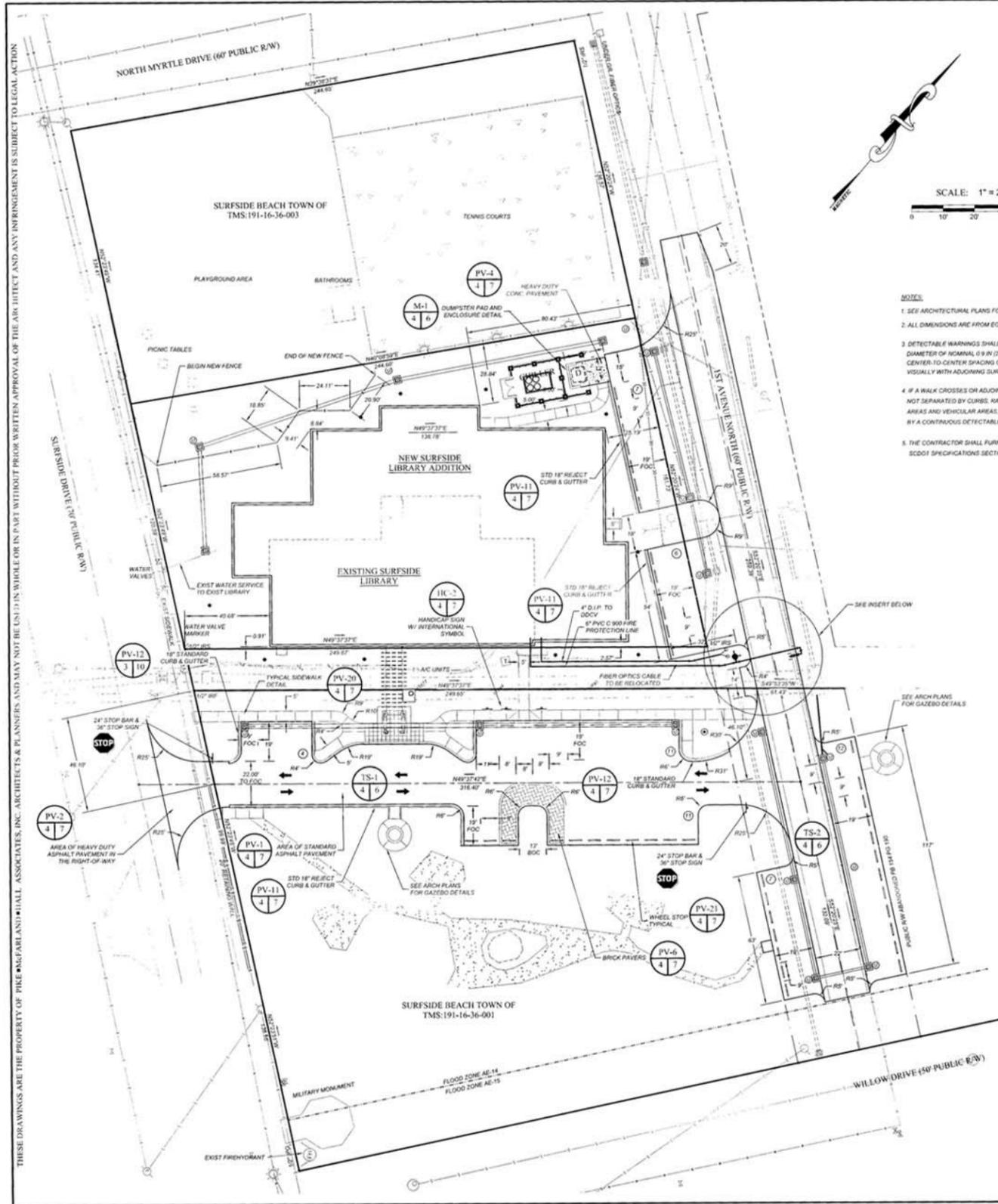
(Exhibit 5.2.6-1)

Currently, the library is located off Surfside Drive, between the passive park, playground and tennis courts. The Town currently has plans to expand the library and provide additional parking as required. PMH Associates, Inc., of Myrtle Beach, South Carolina is developing the design. Also, this area includes a large passive park. The Town plans to close the portion of 1st Avenue North that traverses thru the park, allowing for parking off 1st Avenue North and for the park to be one continuous land area. The Town also plans to construct a small and large Dog Park in this area. Parking will be made available for not only the library, but also for the parks. The Town should continue to pursue these improvements, making this area a more valuable amenity to residents and visitors.

(Image 5.2.6-1)

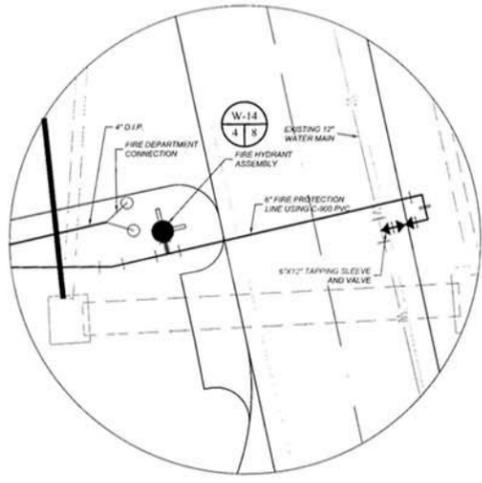


Image 5.2.6-1 Amenity Area



- NOTES:**
1. SEE ARCHITECTURAL PLANS FOR BUILDING DIMENSIONS
 2. ALL DIMENSIONS ARE FROM EOP UNLESS OTHERWISE NOTED
 3. DETECTABLE WARNINGS SHALL CONSIST OF RAISED TRUNCATED DOMES WITH A DIAMETER OF NOMINAL 5.9 IN (151 MM), A HEIGHT OF NOMINAL 0.2 IN (5.1 MM) AND A CENTER-TO-CENTER SPACING OF NOMINAL 2.38 IN (60 MM) AND SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT.
 4. IF A WALK CROSSES OR ADJUNS A VEHICULAR WAY AND THE WALKING SURFACES ARE NOT SEPARATED BY CURBS, RAILINGS, OR OTHER ELEMENTS BETWEEN THE PEDESTRIAN AREAS AND VEHICULAR AREAS, THE BOUNDARY BETWEEN THE AREAS SHALL BE DEFINED BY A CONTINUOUS DETECTABLE WARNING WHICH IS 36 IN (915 MM) WIDE.
 5. THE CONTRACTOR SHALL FURNISH AND INSTALL PAVEMENT MARKING IN ACCORDANCE WITH SCDOT SPECIFICATIONS SECTION 604 AND REQUIREMENTS.

PROJECT DATA	
DEVELOPMENT	LIBRARY BUILDING ADDITION
TMS NO.	191-16-36-003
ZONING	R2
FLOOD ZONE	"AE-14" FEMA MAP NO. 45091C071 H, DATED AUGUST 23, 1999
PROJECT AREA	2.85 AC
LAND DISTURBANCE AREA	1.72 AC
BUILDING AREA W/ EXIST BLDG	11,727 SF
PAVEMENT WITH C&G	32,909 SF
EXISTING PAVEMENT	9,545 SF
SIDEWALK AREA	2,833 SF
PUMPSTER AREA	136 SF
TOTAL IMPERVIOUS AREA	43,196 SF
PERVIOUS AREA	56,900 SF
TOTAL AREA	119,256 SF
IMPERVIOUS PERCENTAGE	50%
SETBACKS	25' MIN. FRONT, 15' MIN. SIDE, 20' MIN. REAR
BUILDING HEIGHT	30'
NUMBER OF BUILDINGS	1
UNITS IN THIS PHASE	N/A
DENSITY	N/A
TOTAL SHG'S PROVIDED	50 SPACES



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PROJECT

ADDITIONS & ALTERATIONS TO:
HORRY COUNTY LIBRARY
SURFSIDE BEACH
SOUTH CAROLINA

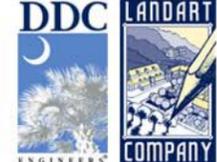
SITE LAYOUT & UTILITY PLAN

CHECKED BY: _____

COMM: 08007
FILE: 127-09-09
DRAWN BY: DCE
PLOT: 1
DATE: 08/25/09
REV: _____

SHEET

C4
4 OF 8



5.3 Future Implementation

1. Sidewalk and Multi-Purpose Path Improvements *(Exhibit 5.3.1-1)*

The Town currently has a sidewalk traversing the eastern portion of Ocean Boulevard from Melody Lane to 16th Avenue North, as well as along several other roads, and has plans to install additional sidewalks in the future. To promote safe and efficient pedestrian travel routes, the Town should link new sidewalks with those that are existing. The Town should also consider installing 8'-10' multi-purpose paths in place of existing sidewalks, especially along roads that experience a high volume of traffic. This would allow for pedestrians, bicycles and golf carts to travel safely throughout the Town. *(Image 5.3.1-1)*

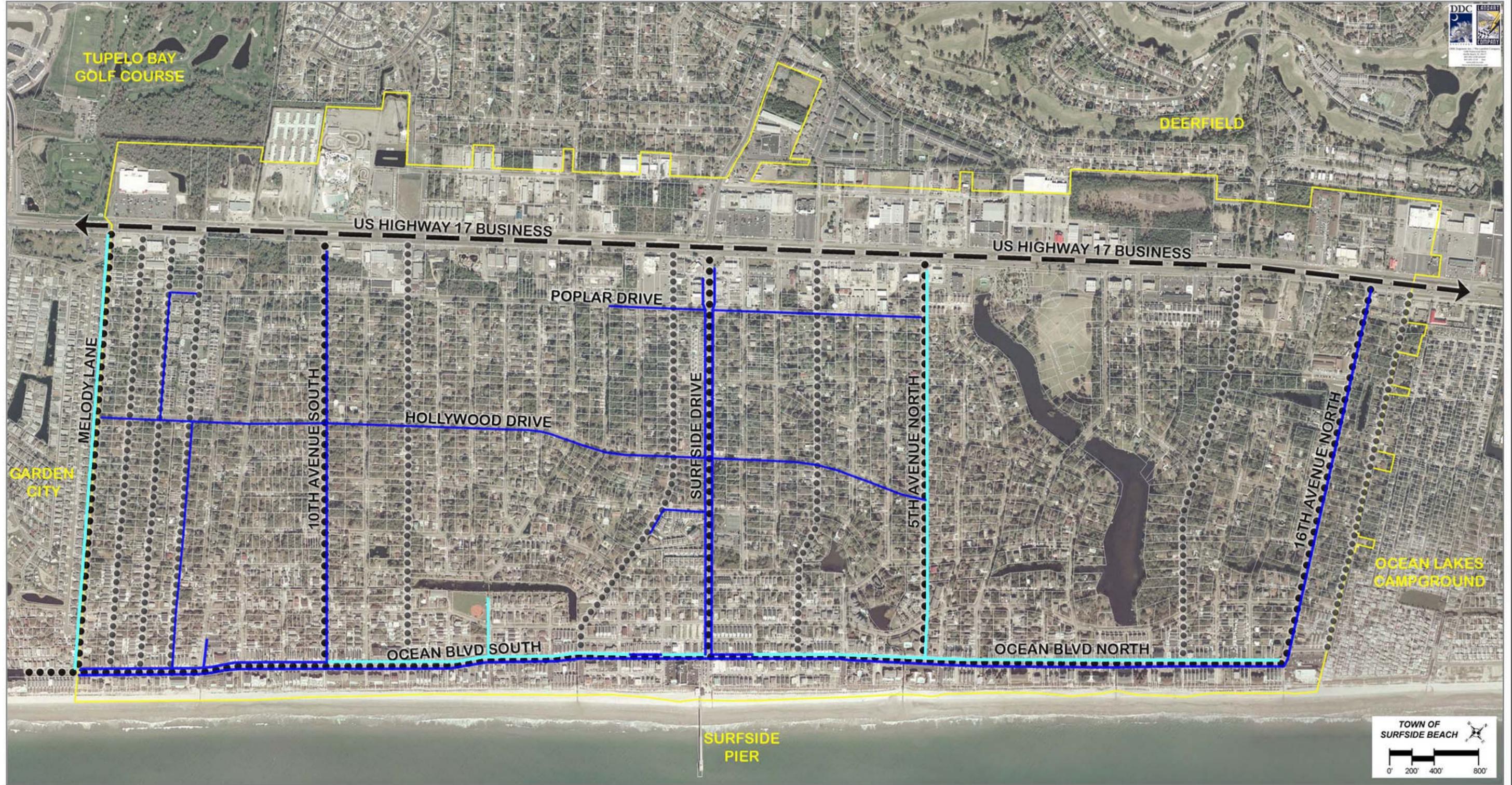


Image 5.3.1-1 Existing Sidewalk

In addition, the Town must establish goals for installing sidewalks along every road in the Town. The Town's Sidewalk Implementation Plan should be used as a guide during this process. There are several other areas where the Town should also focus this effort. A sidewalk currently exists along the western side of Ocean Boulevard from Melody Lane to 10th Avenue South, from 2nd Avenue South to 1st Avenue South and from Surfside Drive to 1st Avenue North. As the streetscape on Ocean Boulevard develops, the Town should continue the sidewalk, or install a multi-purpose path on the western side of the road from 10th Avenue South to 17th Avenue North. A sidewalk or multi-purpose path should also be a main element on at least one side of each of the five (5) "major thru-roads" and from any existing or proposed parking area to Ocean Boulevard. *(Image 5.3.1-2)*



Image 5.3.1-2 Existing Sidewalk



TOWN OF SURFSIDE BEACH, SOUTH CAROLINA
 COMPREHENSIVE PARKING STUDY



KEY	
—	EX. SIDEWALK
—	PROPOSED SIDEWALK

**EXHIBIT 5.3.1-1 PROPOSED
 SIDEWALKS**

2. Parking Control (*Exhibit 5.3.2-1, Chart 5.3.2-1*)

Currently, the Town of Surfside Beach is experiencing vehicular parking in prohibited areas such as in road rights-of-way and along private property. To control and eventually eliminate this problem, it is recommended that bollards be installed on either side of the road in residential areas within close proximity to Ocean Boulevard. A bollard is a short vertical post used to control traffic and prohibit vehicular movement. Bollards are available in many shapes, sizes, heights and materials. The average cost of installing a bollard varies greatly with product choice and installation method; however, if removed with care, many types of bollards may be relocated and reused. (*Image 5.3.2-1, Image 5.3.2-2*)

The Town may also install curb and gutter on all major roads throughout the Town and specifically from 3rd Avenue South to 3rd Avenue North along Ocean Boulevard. Curbing will discourage people from parking in prohibited areas and clearly define vehicular from pedestrian space. Installing curb and gutter in place of open ditches will improve the aesthetics, promote pedestrian safety and eliminate unwanted parking. However, stormwater issues must be handled carefully so as to ensure adequate drainage and, at the same time, maintain water quality standards at an acceptable level. Curb and gutter allows water runoff to be controlled and channeled away from the road and surrounding surface area through a series of underground pipes. This not only creates more usable space along the roadway, but also creates a safer, more aesthetically pleasing streetscape. (*Image 5.3.2-3*)

Another option is to install street trees on either side of the roadways, and specifically along Ocean Boulevard. Street trees or palmettos not only act as barriers to motor vehicles, but also provide shade for pedestrians. The Town could also start a program to install street trees in place of bollards as funds



Image 5.3.2-1 Bollards



Image 5.3.2-2 Bollards



Image 5.3.2-3 Curb and Gutter

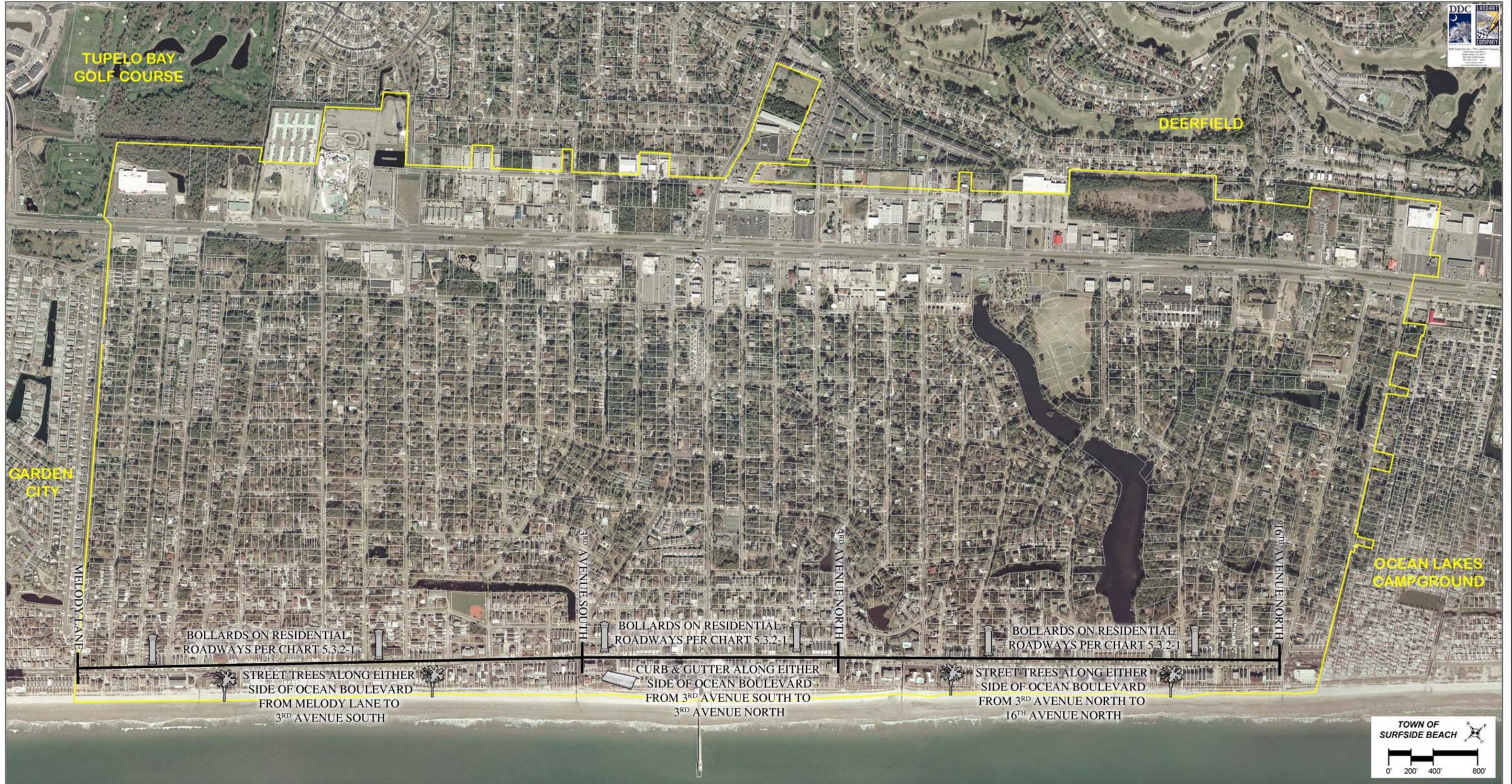


Chart 5.3.2-1 Parking Control Measures

STREET NAME	SOUTH SIDE	NORTH SIDE
MELODY LANE	BOLLARDS AND/OR SIDEWALK	BOLLARDS AND/OR SIDEWALK
16TH SOUTH	BOLLARDS	-
15TH SOUTH	-	BOLLARDS
14TH SOUTH	BOLLARDS	-
13TH SOUTH	BOLLARDS	BOLLARDS
12TH SOUTH	BOLLARDS	BOLLARDS
11TH SOUTH	BOLLARDS	BOLLARDS
10TH SOUTH	BOLLARDS AND/OR SIDEWALK	-
9TH SOUTH	BOLLARDS (WHERE NO DRIVEWAY)	BOLLARDS
8TH SOUTH	BOLLARDS (WHERE NO DRIVEWAY)	BOLLARDS
7TH SOUTH	BOLLARDS	BOLLARDS
6TH SOUTH	BOLLARDS	BOLLARDS AND/OR PARKING PLAN
5TH SOUTH	BOLLARDS	BOLLARDS AND/OR PARKING PLAN
4TH SOUTH	BOLLARDS	BOLLARDS
3RD SOUTH	-	BOLLARDS
2ND SOUTH	-	BOLLARDS
1ST SOUTH	-	-
SURFSIDE DRIVE	-	-
1ST NORTH	-	BOLLARDS
2ND NORTH	BOLLARDS AND/OR PARKING PLAN	BOLLARDS
3RD NORTH	BOLLARDS	BOLLARDS (WHERE NO DRIVEWAY)
4TH NORTH	BOLLARDS	BOLLARDS
5TH NORTH	-	BOLLARDS AND/OR SIDEWALK
6TH NORTH	BOLLARDS	BOLLARDS
7TH NORTH	BOLLARDS	BOLLARDS AND/OR PARKING PLAN
8TH NORTH	BOLLARDS	BOLLARDS
9TH NORTH	-	BOLLARDS
10TH NORTH	BOLLARDS	BOLLARDS
12TH NORTH	BOLLARDS	BOLLARDS (WHERE NO DRIVEWAY)
13TH NORTH	BOLLARDS	BOLLARDS
14TH NORTH	BOLLARDS	BOLLARDS
15TH NORTH	-	BOLLARDS
16TH NORTH	BOLLARDS AND/OR SIDEWALK	-
17TH NORTH	BOLLARDS	-
OCEAN BOULEVARD: MELODY-3RD SOUTH	STREET TREES AND SIDEWALK	STREET TREES AND SIDEWALK
OCEAN BOULEVARD: 3RD SOUTH-3RD NORTH	CURB & GUTTER AND SIDEWALK	CURB & GUTTER AND SIDEWALK
OCEAN BOULEVARD: 3RD NORTH-17TH NORTH	STREET TREES AND SIDEWALK	STREET TREES AND SIDEWALK

become available. The average cost of installing a 14' tall palmetto is approximately \$250.00. An irrigation system is essential to keeping these trees healthy. To ensure that the trees are properly irrigated especially when first planted, the Town can install drip irrigation, water by hand or utilize tree bags, which release water to the tree slowly and require less time and maintenance. Additional landscaping on either side of Ocean Boulevard may create a protective edge between vehicles and pedestrians, as well as prohibit unwanted off street parking and create a softer, more interesting streetscape. (Image 5.3.2-4)



Image 5.3.2-4 Street Trees

3. Shuttle (Exhibit 5.3.3-1)

In an effort to ease traffic congestion during peak periods, the Town may choose to implement a shuttle system utilizing existing roads and parking lots within the Town confines. The Town of Surfside Beach may enter into an agreement with the owner of “Legends in Concert” for the Town to utilize a portion of the “Legends” parking lot, located at the corner of Poplar Drive and 3rd Avenue South, during the day. The location of this parking lot is beneficial in that it is easily accessible from US Highway 17 Business and is proximate to the downtown area. This may prevent residents and visitors from having to drive onto Ocean Boulevard and encourages people to visit shops and/or restaurants in the downtown area. (Image 5.3.3-1, Image 5.3.3-2)



Image 5.3.3-1 Legends in Concert Parking Lot

Upon a mutual agreement between Legends in Concert and The Town of Surfside Beach, the Town could allow “all-day” parking for a set fee and offer a shuttle from the “Legends” parking lot to the Surfside Pier, using 3rd Avenue South and Surfside Drive as a possible route. Because “Legends” show times are generally at 8 pm, with doors opening 1 hour before show time, “all-day” parking could consist of the hours between 8 am and 4 pm. It would also not be necessary to run the shuttle every day of every month. The Town could



Image 5.3.3-2 Legends in Concert Parking Lot



offer the shuttle annually on weekends and holidays between the months of May and September and, if the demand was warranted, the shuttle may then run during the week as well. A shuttle system would alleviate a portion of the vehicular traffic along and surrounding Ocean Boulevard, would allow beach-goers to park all day and would be an additional source of revenue for the Town.

4. Additional Parking Opportunities

There are currently a number of properties within close proximity to beach access points that could potentially become additional parking areas. These properties are recommended based upon overall size, vehicular access, potential number of available spaces and location throughout the Town. It is recommended that the Town purchase these sites to convert into public parking lots.

Specific recommended sites will be provided to the Town Administrator and Town Council members under separate cover during an “Executive Session” of Town Council. We believe that publication of potential sites in this document could create artificial price inflation. Obviously, this would be detrimental to the Town and citizens of Surfside Beach from a financial standpoint.

In addition to the sites to be purchased, there are sites which are owned by the Town that can be improved upon to provide parking. The following sites are recommended for this purpose.

1. Currently, vehicles park on the grass surrounding the Dogwood Drive ball field and play area to use the facilities or to avoid metered parking closer to the beach; however, there is adequate space for the Town to create eight (8±) metered parking spaces along Dogwood Drive and ten (10±) metered parking spaces along 6th Avenue South (*Exhibit 5.3.4-1 Dogwood Drive Ball Field Parking Plan*).

The costs associated with this project would include the cost of parking space construction and meter installation. Defined parking areas would help to control unwanted parking and create a safer environment for pedestrians and, in this case, young children playing in the area. (*Image 5.3.4-1*)



Image 5.3.4-1 Dogwood Drive Ball Field



**EXHIBIT 5.3.4-1 DOGWOOD DRIVE
BALL FIELD PARKING PLAN**

TOWN OF SURFSIDE BEACH, SOUTH CAROLINA
COMPREHENSIVE PARKING STUDY

2. There are currently thirteen (13) paved, un-metered parking spaces available in front of the tennis court and playground area off Surfside Drive, as well as an unpaved area available for vehicles or golf carts to park. The library, located adjacent to the tennis court and playground area, has its own parking lot available for patrons to utilize. The Town currently has plans to expand the library and make other improvements to this area.

Given the growing importance of this area as an amenity, as well as the improvements that are to be made, the Town should provide additional parking. There is adequate space for five (5±) parking spaces to be provided in the unpaved area in front of the tennis courts and playground, which will bring the total parking space count to approximately eighteen (18±) in this area (*Exhibit 5.3.4-2 Surfside Drive Amenity Area Parking Plan*). Additional parking for the passive and dog parks should also be provided directly off of 1st Avenue North. Because the plans for this area include the closure of part of 1st Avenue North, there is a great opportunity to create even more parking. The parking spaces could be metered during peak periods, to assist in controlling parking in this area and to generate revenue for the Town, and un-metered during the off season, to allow residents to take advantage of the amenity area without having to pay a parking fee. The costs associated with this project would include the cost of parking space construction and meter installation. (*Image 5.3.4-2*)

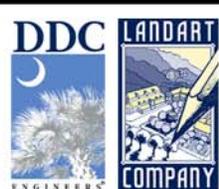


Image 5.3.4-2 Surfside Drive Amenity Area

3. Although the two buildings are located adjacent to each other, the Surfside Pier and the building which houses the River City Café and Latitude 22 restaurants do not share a common parking lot. The Surfside Pier parking lot currently includes forty-six (46) available metered spaces; the Yaupon Drive parking lot, located across Ocean Boulevard and two blocks back from the pier, includes sixty-four (64) available metered spaces. Both lots are consistently overcrowded during peak periods. In order to relieve congestion in this area, the Town may enter into an agreement with the owner of the River City Café and



Image 5.3.4-3 River City Cafe Parking Lot



**EXHIBIT 5.3.4-2 SURFSIDE DRIVE
AMENITY AREA PARKING PLAN**
TOWN OF SURFSIDE BEACH, SOUTH CAROLINA
COMPREHENSIVE PARKING STUDY

Latitude 22 restaurant building to utilize the building's parking lot (*Exhibit 5.3.4-3 River City Cafe Shared Parking Plan*). Parking could be metered or the Town could charge a set fee to park in this area. Entering into this agreement would provide the Town with an additional forty (40±) parking spaces in one of the Town's busiest parking areas. (*Image 5.3.4-3*)

4. At the intersection of 3rd Avenue South and Dogwood Drive, there is currently an unused area of grass surrounding the banks of Floral Lake. Currently, vehicles park in this area to avoid metered parking elsewhere. To take advantage of this potential revenue, the Town should acquire the land to create approximately eleven (11±) metered spaces (*Exhibit 5.3.4-4 3rd Avenue South Parking Plan*). Costs associated with this project would be the cost of acquiring the land, parking space construction and meter installation. (*Image 5.3.4-4*)



Image 5.3.4-4 3rd Avenue South



**EXHIBIT 5.3.4-3 RIVER CITY CAFE
SHARED PARKING PLAN**

TOWN OF SURFSIDE BEACH, SOUTH CAROLINA
COMPREHENSIVE PARKING STUDY



EXHIBIT 5.3.4-4 3RD AVENUE SOUTH PARKING PLAN

TOWN OF SURFSIDE BEACH, SOUTH CAROLINA
COMPREHENSIVE PARKING STUDY

5. Bicycle Circulation Improvements

The Town of Surfside Beach should continue to provide safe bicycle parking and offer bicycle racks in heavily utilized places such as beach access points, parks and commercial areas within the Town. With the addition of multi-purpose paths and/or bicycle lanes, people may be more inclined to use bicycles. Signage is also important to make vehicular traffic aware that bikers may be on the road. “Share the road” and “bike lane” signs and symbols should be included in the overall signage design process. (Image 5.3.5-1)



Image 5.3.5-1 Existing Bicycle Rack

The East Coast Greenway is a proposed series of interconnected bicycle trails stretching over 2000 miles from Maine to Florida. There is an Alliance that oversees the effort, but it is up to individual states and municipalities to build and maintain the trails. The Town of Surfside Beach has a great opportunity to take part in this initiative and connect with other areas up and down the East Coast. After studying where sidewalks and multi-purpose paths exist or have been suggested, the recommended route through the Town is from US Highway 17 Business to 16th Avenue North, to Ocean Boulevard, to Melody Lane and back onto US Highway 17 Business (Exhibit 5.3.5-1 Proposed Bicycle Route). Because the East Coast Greenway effort is already underway in many surrounding areas, the Town may risk losing the opportunity to identify its own route if future planning and streetscape goals do not include bicycle transportation. (Image 5.3.5-2)



Image 5.3.5-2 East Coast Greenway Route

On a smaller scale, the Town may connect to trails designated by the City of Myrtle Beach and coordinate with Garden City to develop plans to connect to trails designated by Murrells Inlet. Interconnected trails could even lead to “Grand Strand” bike marathons and cooperative area events that could bring additional revenue to the Town in the future.



6. Improvements Master Plan

An overall Improvements Master Plan should be an integral part to the Town of Surfside Beach's improvement efforts. The document should reflect cooperative input from Town Council, the Beautification Advisory Committee, residents and visitors, and should be easily available for public view and used and enforced with regards to all future improvement decisions. The master plan should outline the Town's overall vision, including short-term and long-term goals, and break each project into an order of importance and timeframe. It is also important for the Town to consider existing area studies and future plans of surrounding municipalities to take advantage of opportunities that might indirectly benefit the Town.

Several key areas should be included in the Town's improvements master plan and they are the following:

- An agreed upon overall "signature look" for the Town;
- Streetscape improvements listed for each road, including sidewalk installation, repaving, addition of street trees or bollards, etc;
- Overall plan to improve drainage, pipe ditches, etc and create more usable streetscape space;
- Existing and proposed parking plan;
- Hardscape and complementary signage improvements; and
- East Coast Greenway bicycle route.

An Improvements Master Plan will become a concrete plan and timeline with which the Town can move forward. The creation of an Improvements Master Plan will also assist in preventing fragmented projects within the Town and ensure continuity, Promote Smart Growth and help keep track of funds and ongoing projects. (Image 5.3.6-1, Image 5.3.6-2)



Image 5.3.6-1 Existing Signage



Image 5.3.6-2 Current Town Project

7. Retail Space and Parking Garage

To better utilize land available for parking, the Town may consider constructing a structured parking deck. This type of parking would work well in the area of Town closest to Surfside Pier, at the intersection of Surfside Drive and Ocean Boulevard (*Exhibit 5.3.7-1 Retail Space and Parking Garage Plan*). The proximity to the beach, pier, restaurants and commercial establishments will assist in creating this area into a thriving focal point for residents and visitors to enjoy.

A two (2) or three (3) story parking deck located in this area would provide much needed additional parking spaces in a currently overcrowded area. With a location on the south side of Surfside Drive, a parking deck would provide optimal ingress/egress for vehicles and ease of access to the pier, restaurants and beach for pedestrians. The first floor of the deck could be fronted by commercial and / or restaurant space to expand business opportunities in this area of the Town and create an active pedestrian streetscape. (*Image 5.3.7-1, Image 5.3.7-2*)



Image 5.3.7-1 Parking Garage with Retail Facade



Image 5.3.7-2 Parking Garage with Retail Facade



DOGWOOD DRIVE

PARKING GARAGE
LOCATION

SURFSIDE DRIVE

YAUPON DRIVE



RETAIL/RESTAURANT
FRONTS

OCEAN BOULEVARD

SURFSIDE PIER
PARKING
LOT



EXHIBIT 5.3.7-1 RETAIL SPACE AND PARKING GARAGE PLAN

TOWN OF SURFSIDE BEACH, SOUTH CAROLINA
COMPREHENSIVE PARKING STUDY

6. RECOMMENDED COURSE OF ACTION

6.1 Introduction

The Town of Surfside Beach, South Carolina has identified parking as one (1) of the top issues currently adversely affecting the Town. Overall, the Town has thirty-three (33) beach access points and twelve (12) existing areas, located from Melody Lane to 17th Avenue North, which are dedicated to providing beach parking. The Town currently utilizes a variety of signage, ticketing and metered spaces in an effort to control parking, however, vehicular circulation and parking continually and consistently present challenges for the Town, especially during the summer tourist season.

The intent of this section is to provide a Working Plan, separated into individual Phases, that provide guidance to the Town of Surfside Beach in the effort to relieve parking problems (*Exhibit 6.1-1 Course of Action-Phasing Plan*). Each Phase is further subdivided into steps which, based on the findings of this report, are listed in a general order of actions which, based upon the professional opinion of the DDC and LandArt Staff, should be administered. We believe that implementation of the Plan will ultimately assist the Town to better organize, utilize and benefit from its available assets.

6.2 Phase One

The first step in undertaking the Town of Surfside Beach's parking problem is to better organize and utilize the Town's existing transportation infrastructure and parking areas. Phase One of this Plan includes immediate steps that the Town should take, while utilizing and improving upon resources that currently exist.

1. Towing vs. Ticketing

Currently, the Town issues tickets to parking offenders, which may deter vehicles from parking in prohibited areas; however, there are still vehicles that consistently park illegally throughout the Town limits. Given this, it is recommended that the Town begin to tow and/or boot cars parked in prohibited areas rather than issue tickets. Both methods send stronger messages to owners of illegally parked vehicles. This will significantly reduce the likelihood of repeat offenders. Towing will also generate more revenue for the Town in the short term, as it costs a great deal more

to recover a towed vehicle than to pay a relatively small parking fine. (*Section 2.3*)

2. Parking Control

The Town currently has problems with vehicles parking along either side of Ocean Boulevard and either side of various residential roads within close proximity to beach accesses. This creates a hazard for pedestrians and other vehicles utilizing the roadways and sidewalks, while diluting potential parking revenue to the Town. It is recommended that the Town utilize a variety of parking control methods, as recommended in chart 5.3.2-1 (Parking Control Measures), to prohibit vehicles from parking in "non-designated areas", thereby forcing vehicles to park in metered spaces. (*Section 5.3.2*)

Bollards should be installed along residential roads within close proximity to Ocean Boulevard. Permanent and / or removable bollards may be used, depending upon the desired time of use and location of each bollard. Bollards should be spaced between ten (10) and fifteen (15) feet on-center upon installation in order to prevent vehicles from parking between them. Bollards may also be used on either side of Ocean Boulevard in lieu of street trees, until funding for street trees is available.

Street trees, or Palmettos, should be installed on either side of Ocean Boulevard where a sidewalk exists away from the road, specifically from Melody Lane to 3rd Avenue South and from 3rd Avenue North to 17th Avenue North. The trees should be installed in the area between the roadway and the sidewalk or multi-purpose path, and should be spaced between ten (10) and fifteen (15) feet on center upon installation. An irrigation system, whether use of a drip system, gator bags, or watering by hand should also be set in place immediately upon installation of the trees. Street trees along Ocean Boulevard will not only prohibit vehicles from parking on either side of the road, but will also create a pleasant streetscape, define vehicular space and provide shade for pedestrians. Additional landscaping, such as shrubs and groundcover, should also be installed in the area between the roadway and the sidewalk; however, plant material should be maintained so that pedestrians can move safely and be seen from Ocean Boulevard.

PROJECTED TIME FRAME	PHASE AND RECOMMENDED COURSE OF ACTION
PRESENT	EXISTING MEASURES CONTINUE TO TICKET VEHICLES UTILIZE METERED PARKING DEVELOP SIGNAGE
2010-2011	PHASE ONE (REPORT SECTION 6.2) 1. TOW VEHICLES 2. INSTALL PARKING CONTROL METHODS BOLLARDS - STREET TREES - CURB & GUTTER 3. IMPROVE / MAINTAIN METERED PARKING 4. SIMPLIFY & UNIFY SIGNAGE CREATE ADDITIONAL PARKING 5. POPLAR DRIVE AND SURFSIDE DRIVE 6. LIBRARY / AMENITY AREA
2011-2015	PHASE TWO (REPORT SECTION 6.3) 1. DESIGNATE MAJOR THRU-ROADS HARDSCAPE - STREETSCAPE 2. INITIATE SIDEWALK IMPLEMENTATION PLAN
WHEN APPLICABLE	PHASE THREE (REPORT SECTION 6.4) 1. INSTITUTE 40/60 SPLIT OPTION 2. NEGOTIATE USE OF RIVER CITY CAFE PARKING LOT CONVERT EXISTING LAND OWNED BY THE TOWN INTO PARKING 3. DOGWOOD DRIVE BALL FIELD 4. 3RD AVENUE SOUTH AND DOGWOOD DRIVE
AFTER PHASE THREE	PHASE FOUR (REPORT SECTION 6.5) 1. DEVELOP SHUTTLE SERVICE
FUTURE	PHASE FIVE (REPORT SECTION 6.6) 1. CONVERT LAND LEASED / PURCHASED BY THE TOWN INTO ADDITIONAL PARKING
FUTURE	PHASE SIX (REPORT SECTION 6.7) 1. CONSTRUCT PARKING GARAGE



There are a number of businesses and restaurants along Ocean Boulevard in the area closest to the Surfside Pier. Because this area is one of the most congested areas within the Town limits, it is recommended that the Town install curb and gutter where it does not currently exist along Ocean Boulevard, from 3rd Avenue South to 3rd Avenue North. Curb cuts into existing business parking areas should be limited, or eliminated where other access is available, and a sidewalk should be installed along the roadway to help better define vehicular and pedestrian spaces.

3. Metered Parking

The Town has provided metered spaces in parking locations within close proximity to beach accesses. However, these spaces are not fully utilized currently. It is recommended that the Town take steps to ensure that existing parking is utilized to the fullest extent. All parking should be easily accessible and perceived as a safe environment for both vehicles and pedestrians. To achieve this, it is important for the Town to ensure that all parking areas are well-maintained. Waste should be picked up frequently, parking surfaces should be well-kept, surrounding vegetation should be maintained to appropriate sizes and sidewalks and lighting should be provided where possible. (Section 5.2.2)

It is also recommended that the Town take steps to guide and effectively “force” vehicles into designated parking areas thru the use of parking control and directional signage. The installation of parking control devices and restructuring of the Town’s signage should occur simultaneous with or shortly after improvements to the Town’s existing parking areas are made.

4. Signage

The Town currently utilizes signage for a variety of reasons. This includes displays of important information, beach and park rules, upcoming events and parking regulations. Signs of various sizes, shapes and colors are located throughout the Town and are often clustered together, making them difficult to interpret while one is concentrating on the

task of driving. In areas within close proximity to Ocean Boulevard, the most frequently used sign is “No Parking”; however, signs that indicate where parking is available are either not noticeable or not present.

It is recommended that the Town simplify and unify all signage within the Town limits. The Town should develop a cohesive signage system which will clearly guide vehicles to metered parking areas, while parking control methods, described in Step 2 (Parking Control), will take the place of negative “No Parking” signage. All designated parking areas should also be clearly identified with the same simple sign, readable from the roadway. (Section 4.5)

5. Poplar Drive and Surfside Drive

The Town currently has plans to develop the corner of Surfside Drive and Poplar Drive, located at the edge of the downtown area, into a parking lot and passive park space. Although this parking area would not be metered, it would spark the enhancement of the downtown area and provide valuable off-street parking in the event that Surfside Drive is redeveloped. Because construction drawings for this project have already been initiated, it is recommended that the Town follow thru on completing the parking lot and park project in this area. (Section 5.2.5)

6. Library / Amenity Area

The Town currently has plans to expand the library and amenity area, located at the corner of Surfside Drive and Myrtle Drive. There is existing parking proximate to the tennis courts and playground, however; there is adequate space for additional parking. Because the Town already owns this property, it is recommended that the Town provide additional parking, where possible, as part of the expansion process. (Section 5.2.6, Section 5.3.4-2)

6.3 Phase Two

To help filter traffic throughout the Town and ease congestion in the Surfside Drive area, it is recommended that the Town designate major thru-roads from US Highway 17 Business to Ocean Boulevard, as specified in Section 2.4.1 (Vehicular Traffic). These roads should be clearly identified to vehicles traveling along US Highway 17 Business as roads that offer a direct route to all beach access points. In order to implement this program, the Town should make efforts to have the South Carolina Department of Transportation (SCDOT) install a traffic signal at the intersection of US Highway 17 Business and 10th Avenue South, as well as at the intersection of US Highway 17 Business and 16th Avenue North. Hardscape, such as a subtle low entrance wall, should be installed to announce a major thru-road. Sidewalks or multi-purpose paths should be installed, if not already existing, along major thru-roads as well to allow for pedestrian traffic. (Section 4.2)

In addition to designating and improving major thru-roads, it is recommended that the Town follow thru with the Sidewalk Implementation Plan. Installing sidewalks along roadways that experience a high volume of traffic creates a safer environment for pedestrians and encourages people to use other forms of transportation rather than vehicles, thereby easing some congestion along the Town's roadway system. (Section 5.3.1)

6.4 Phase Three

Once the Town of Surfside Beach has fully utilized its existing resources, the next step in addressing the Town's parking problem is to expand the amount of parking available. Phase Three of this Plan includes existing available land owned by the Town which should be converted into additional parking, as well as suggested agreements that should be made between the Town and local property owners.

1. 40/60 Split Option

There are several undeveloped oceanfront properties located throughout the Town. Many of these lots are currently vacant because of the current state of the economy, as well as an assortment of other reasons. In instances where the property owners may not have plans to build in the foreseeable

future, it is recommended that the Town implement a program to split the costs and benefits from creating a public parking area. The program would allow the Town to provide much needed additional parking proximate to existing beach access points, while generating income for the Town and the property owner for an agreed upon period of time.

2. River City Café Shared Parking Lot

The River City Café parking lot is located adjacent to the Surfside Pier parking lot and is currently reserved for patrons of the restaurants. During the peak tourist season, the Surfside Pier parking lot is perhaps the busiest and most congested area within the Town limits. Because of this, it is recommended that the Town negotiate with the owner of the River City Café parking lot to allow the Town to utilize the lot for beachgoers. The Town's parking revenue would increase from additional available spaces, which could then be used, in part, to compensate the parking lot owner. (Section 5.3.4-3)

3. Dogwood Drive Ball Field

There is currently no designated parking in the area surrounding the Dogwood Drive ball field, however, vehicles consistently park next to the road in this area. Because the Town already owns this property, there is an opportunity to develop parking at this location, and thereby create additional revenue thru metered spaces. (Section 5.3.4-1)

4. 3rd Avenue South and Dogwood Drive

Similar to the current situation in the ball field area, vehicles consistently park next to the road near the intersection of Dogwood Drive and 3rd Avenue South. Because the Town also already owns some of this property, it is recommended that the Town provide metered parking in this area as well. (Section 5.3.4-4)

6.5 Phase Four

Once land currently owned by the Town has been fully utilized for parking, the Town should create a shuttle system to further relieve congestion on roadways within close proximity to beach access points. It is recommended that the Town negotiate with the owner of the Legends in Concert parking lot to allow the Town to utilize the lot for shuttle parking. A shuttle system, as described in Section 5.3.3 (Shuttle), is an efficient method to transport beach-goers from a parking area to a beach access point, while reducing vehicular traffic and congestion. *(Section 5.3.3)*

6.6 Phase Five

As described in the previous Phases, there are a variety of steps the Town of Surfside Beach may take to improve the current transportation and parking situation. After taking full advantage of existing parking opportunities, the Town should look towards expanding its assets. Phase Five of this Plan includes the recommendation that the Town purchase properties as presented to the Town Administrator and Town Council, to convert into additional parking. *(Section 5.3.4)*

6.7 Phase Six

The final step in this phased approach to provide parking is for the Town to construct a parking garage proximate to the intersection of Ocean Boulevard and Surfside Drive. The vision for this project includes a garage with storefronts facing the major roads. This scenario would provide the most additional parking within the smallest amount of land space, and would also create an interesting pedestrian space that would enhance the existing streetscape. This project would require the greatest cost; however, the impact upon its completion would be the most dramatic and beneficial. *(Section 5.3.7)*